



**CITY OF AVON LAKE  
PLANNING COMMISSION  
150 Avon Belden Road  
Avon Lake, Ohio 44012  
(440) 930-4110**

Voting Order

J. Leitch  
H. Ma  
S. Orille  
C. Raymond  
G. Smith  
M. Spaetzel  
R. Haas

**AGENDA  
Tuesday, August 5, 2025  
7:00 PM  
City Council Chambers**

**ROLL CALL:**

Mr. Haas, Mr. Leitch, Dr. Ma, Mr. Orille, Mrs. Raymond, Mr. Smith, Mayor Spaetzel, Director of Law Ebert, Engineer Howard, Community Development Director Esborn, and Planning and Zoning Manager La Rosa.

- 1. APPROVAL OF JULY 1, 2025, MEETING MINUTES**
- 2. GENERAL CORRESPONDENCE/ANNOUNCEMENTS**
- 3. COUNCIL REPORT**
- 4. SWEARING IN PUBLIC COMMENTERS**
- 5. TABLED CASES:**

**Case No. CPC-25-9**, Pulte Homes of Ohio, LLC, Major Subdivision Preliminary Plat Approval for Harbor Crest Townhome Subdivision, located in the southeast corner of Walker Road and Avon-Belden Road (SR 83).

Applicable Code Sections: 1214.05: Major Subdivision and 1238: Subdivision Design Standards apply.

- 6. NEW CASES**

**Case No. CPC-25-11**, Ford Motor Company, Ohio Assembly Plant, Site Plan Approval for the Construction of a New Vehicle Test Track and Installation of a New Stormwater Retention Basin.

Applicable Code Section: 1214.06 Site Plans apply.

- 7. OTHER BUSINESS**
- 8. GENERAL PUBLIC COMMENT**
- 9. ADJOURNMENT**

The next regular meeting of the Planning Commission is Wednesday, September 3, 2025.

**MINUTES OF THE AVON LAKE**  
**PLANNING COMMISSION MEETING**  
**July 1, 2025**

A regular meeting of the Avon Lake Planning Commission was called to order on July 1, 2025, at 7:00 P.M. in Council Chambers with Chairperson Ma presiding.

**ROLL CALL**

Present for roll call were Mr. Haas, Dr. Ma, Mr. Leitch, Mr. Orille, Mrs. Raymond, Mr. Smith, Mayor Spaetzel, Director of Law Ebert, City Engineer Howard, Community Development Director Esborn, and Planning and Zoning Manager La Rosa.

**APPROVAL OF MINUTES**

Mr. Haas moved, and Mrs. Raymond seconded to approve the June 3, 2025, meeting minutes as presented. Motion carried (7-0).

**GENERAL CORRESPONDENCE & ANNOUNCEMENTS**

Email from Lori Glenn of 32069 Kossuth Drive objecting to the Handford Blvd extension, citing traffic, safety, and transparency concerns.

Email from Helen Zdrojewski of 621 Brust Drive regarding the unfulfilled MOU for gate installation at Handford Blvd, tied to the Port Side development.

No Planning Commission action is required; correspondence was included in the Commission's packets.

**COUNCIL REPORT**

Mr. Smith advised that the Avon Center Estates preliminary plat was approved, and the final plats for Sandridge and Port Side Townhomes had their first reading.

**SWEARING IN PUBLIC COMMENTERS**

Law Director Ebert administered the oath to all individuals providing public comments during the meeting.

Dr. Ma announced a procedural adjustment to enhance transparency and public participation. Beginning with this meeting, public comments for each case will be accepted after the Planning Commission's discussion and questions, but before any motion or vote.

Dr. Ma explained that under the previous format, public comment was reserved until the end of the meeting, which limited the public's ability to provide timely input on specific agenda items before decisions were made. By repositioning the public comment period within the order of business—following commissioner discussion but before action is taken—residents and stakeholders would have a more meaningful opportunity to contribute to the deliberation process. She emphasized that this change applies specifically to comments related to individual cases on the agenda, while general public comments would still be welcomed at the end of the meeting.

**NEW CASES**

**Case No. CPC-25-9, Pulte Homes of Ohio, LLC, Major Subdivision Preliminary Plat Approval for Harbor Crest Townhome Subdivision**, located in the southeast corner of Walker Road and Avon-Belden Road (SR 83). Applicable Code Sections: 1214.05: Major Subdivision and 1238: Subdivision Design Standards apply.

James Sayler of The Henry G. Reitz Engineering Company, representing Pulte Homes of Ohio, presented the preliminary subdivision plat for the proposed Harbor Crest Townhomes, located at the southeast corner of Walker Road and Avon-Belden Road (SR 83). Mr. Sayler was accompanied by Keith Filipkowski from Pulte Homes and confirmed that both had been sworn in for testimony.

Mr. Sayler began by noting that this project was not new to the Planning Commission, having previously appeared in work session format and as part of a rezoning request. He explained that feedback received from both the Administration and the Commission during those earlier meetings was incorporated into the current preliminary plat. The plat presented maintained the same layout and unit count as the prior concept but included more detailed engineering and design documents.

Mr. Sayler described the project as a 73-unit townhome development that would be constructed in two phases. Phase 1 would begin from the Avon-Belden Road (SR 83) entrance and include lots 1 through 42. Phase 2 would extend south and connect to Walker Road. The lot numbering was updated to reflect the phasing sequence.

Mr. Sayler highlighted several plan features:

- **Drainage Improvements:** In response to concerns raised by adjacent property owner Mr. Wolfe, the plan includes enhanced drainage design near his property to prevent stormwater runoff from the new development.
- **Recreational Space:** A pocket park and hardscape gathering area, shown previously during the rezoning process, was now formally included in the plan.
- **Traffic Analysis:** A traffic study has been completed and reviewed by an independent consultant retained by the city. While the study noted no current conflicts, it suggested relocating the development's entrance on SR 83 to better align with Huntington Bank's northern curb cut. Mr. Sayler stated that shifting the entrance could negatively affect the design and reduce space available for stormwater detention and recommended retaining the current location.
- **Engineering Review:** Mr. Sayler acknowledged comments from City Engineer Howard, including a request to conduct an auto-turn analysis for safety vehicle access, a lot consolidation review, and feedback on the traffic study. He committed to providing additional information as needed during the improvement plan phase.
- **Zoning Comments:** Mr. Sayler addressed two outstanding items, including establishing an easement or access plan for potential future connectivity at the southern stub street and reconsideration of a second exit lane for left-turning vehicles onto Avon-Belden Road, which Pulte preferred not to include due to limited space and minimal traffic impact.
- **Landscape and Assembly Plans:** A landscape plan by the EDGE Group was submitted and included in the application. Mr. Sayler noted that the applicant had resubmitted an updated lot assembly plan for engineering review and was in the process of finalizing deeds for property consolidation before construction.

Mr. Sayler concluded by reaffirming Pulte's willingness to collaborate with the city to resolve outstanding technical details and respond to any further questions from staff or Commission members.

Planning Commission comments focused on the following:

- **Entry Drive Alignment [Avon-Belden Road (SR 83) / Huntington Bank]:** Concerns were raised about the proposed entrance on Avon-Belden Road (SR 83) not aligning with Huntington Bank's drive. It was noted that misaligned curb cuts could lead to future traffic conflicts, especially with left-turning vehicles entering or exiting either site. There was discussion about whether the entrance could be moved slightly south or otherwise to improve spacing and safety.
- **Lack of Left-Turn Exit Lane [Avon-Belden Road (SR 83)]:** The absence of a dedicated left-turn lane for vehicles exiting onto Avon-Belden Road (SR 83) was identified as a potential cause of traffic congestion and safety concerns. It was noted that this could result in delays, stacking, and future complaints from residents. Additionally, once construction is complete, the city may have limited options to mitigate these issues if a turn lane is not incorporated during development.
- **Southern Stub Street Connection:** Considerable discussion focused on whether the southern stub street should remain open for future interconnection or be closed. Concerns were raised about potential complications based on past experiences with unresolved or disputed private street connections. Some suggested that the stub be closed and landscaped, unless or until a formal agreement for future access is reached.
- **Parking Availability and On-Street Parking Limitations:** Concerns were expressed regarding limited guest parking, particularly near the Walker Road end of the site. Questions were posed regarding the potential for residents to improperly store vehicles in common areas, such as near the mailbox cluster. It was noted that the 22-foot-wide private streets do not accommodate on-street parking, further limiting available space.
- **Comprehensive Plan Consistency / Mixed-Use Vision:** Questions were raised about whether the proposal fully aligns with the city's Comprehensive Plan, which envisions a mixed-use, walkable Town Center. While the plan encourages increased housing diversity and density, it also emphasizes integration with retail or civic uses, elements not included in the Harbor Crest proposal. Director Esborn clarified the distinction between the general mixed-use land use designation and the formal Mixed Use Overlay District, which was not applied to this site.
- **Stub Street Circulation / Waste Collection:** Inquiries were made about how waste collection vehicles would service the units along the stub street and whether adequate turning space is provided. The applicant responded that no turnaround is proposed, noting this approach is consistent with how similar stub streets are currently serviced in Avon Lake.
- **Lighting Plan Details:** The Commission asked about the provision of street lighting along the private roads. Mr. Filipkowski stated that lighting would be coordinated with FirstEnergy and supplemented by coach lights on the exterior of each unit. However, a detailed photometric plan was not presented.
- **Neighbor Coordination / Landscape Buffering:** Inquiries were made about outreach to adjacent property owners, particularly those along the shared property lines. Mr. Filipkowski reported having positive communication with Mr. Wagner and a willingness to coordinate fence and landscaping placement. At the time of the meeting, no outreach occurred with the other nearby property owners.

- **Phasing and Infrastructure Timing:** Clarification was requested on the phasing plan and utility extensions. Mr. Sayler explained that the phasing reflects typical build-out timelines and noted that some infrastructure details would be addressed during final engineering.
- **Clarification of Traffic Study Terminology:** It was suggested that future traffic study submissions include a legend or key to clarify abbreviations and technical terms, making the documents easier to interpret.

William Zimmerman (resident) was in opposition to the plan, citing traffic and walkability concerns.

Mr. Haas moved, and Mr. Leitch seconded to table Case No. CPC-25-9, Pulte Homes of Ohio, LLC, for Preliminary Plat Approval of the Harbor Crest Townhome Subdivision, to allow the applicant time to revise and resubmit the plan. The Commission requests that the following items be addressed and presented at the next regular Planning Commission meeting:

1. A revised entry configuration at Avon-Belden Road (SR 83) that explores one or more of the following: aligning the development entrance with Huntington Bank's drive, shifting the curb cut further south to improve spacing from adjacent access points, or adding a left-turn exit lane to improve outbound vehicle circulation and minimize queueing.
2. Clarification regarding the proposed stub street connection at the southern end of the site, including whether it will remain open for future connection or be terminated with appropriate landscaping or buffering, and how this will be addressed in the HOA governing documents.
3. Confirmation that the applicant will meet with the Development Review Committee (DRC) before resubmission, to address staff and departmental concerns and ensure coordinated review of the revised plan.

Motion carried (7-0).

## **OTHER BUSINESS**

### **Proposed Right-of-Way Vacations – Electric Boulevard:**

A proposal to vacate portions of the Electric Boulevard right-of-way adjacent to properties owned by Anthony and Kimberly Calabrese and the Smugglers Cove Condominium Complex No. 4. The proposed vacation includes approximately 0.1505 acres (6,556 square feet) to be incorporated into the Calabrese property and approximately 0.1567 acres (6,824 square feet) to be incorporated into the Smugglers Cove parcel. Applicable Code Section: 1216.03 (c) Zoning District Map and District Boundaries, Vacation of Public Rights-of-Way applies.

The applicant, Anthony Calabrese, requested the vacation of an undeveloped portion of Electric Boulevard right-of-way, totaling approximately 0.3072 acres, to be split between his property and the adjacent Smugglers Cove Condominium Complex. Mr. Calabrese stated he intended to consolidate these parcels and build a single-family home for his family.

Director Esborn and Engineer Howard confirmed the right-of-way was unimproved and not needed for public access or utility purposes.

No opposition from other Commission members was recorded, and no additional technical concerns were raised by staff.

Mr. Smith disclosed that he had previously represented Mr. Calabrese and also owned property in Smugglers Cove. He recused himself from participating in or advising on this case.

Mr. Leitch moved, and Mayor Spaetzel seconded to approve Mr. Smith's request to abstain from voting

due to a potential conflict of interest. Motion carried: (6-0). Mr. Smith abstained from the vote.

Jennifer Fenderbosch (resident) raised a question about whether property owners seeking rights-of-way vacations should be required to pay the City for the land being vacated. She cited past examples during former Councilman Rob James's tenure where such compensation had been required and suggested that the City Clerk research historical precedent.

Amy Adams (Smugglers Cove resident) requested that, in addition to researching prior cases involving payment, a list showing instances where no compensation was required be provided to ensure a balanced view.

Mr. Haas moved, and Mr. Leitch seconded to vacate portions of the Electric Boulevard right-of-way adjacent to properties owned by Anthony and Kimberly Calabrese and the Smugglers Cove Condominium. Motion carried (6-0), with Mr. Smith abstaining.

### **Proposed Right-of-Way Vacation – Shields Avenue and Electric Boulevard**

A proposal from Avon Lake Regional Water to vacate a 15.15-foot-wide portion of the Shields Avenue right-of-way and a portion of the Electric Boulevard right-of-way. The proposed vacation areas are intended to be incorporated into the Avon Lake Regional Water property for operational purposes. Applicable Code Section: 1216.03 (c) Zoning District Map and District Boundaries, Vacation of Public Rights-of-Way applies.

Director Esborn explained that this request was part of a minor subdivision and had been reviewed twice by the Development Review Committee. The land in question was formerly associated with the power plant property and was acquired by Avon Lake Regional Water for operational expansion. Engineer Howard confirmed the vacation involved paper streets not currently in use. He supported the request as a cleanup measure for the unused right-of-way. Mr. Haas asked whether the vacation would affect emergency access or connectivity. Director Esborn responded that fire and safety services had reviewed the proposal and expressed no concerns. No objections were raised by the other Commissioners. All acknowledged the vacation as appropriate for utility operations and not needed for future city infrastructure.

Jennifer Fenderbosch (resident) reiterated her earlier point from the Electric Boulevard vacation case, questioning whether the city should consider requiring payment or compensation when vacating public right-of-way, especially for large utility operators. She suggested that the Council Clerk could provide records of past vacations that involved reimbursement.

William Zimmerman (resident) requested a visual depiction of the exact area being vacated. Mr. Zimmerman was provided the map for clarification.

Mr. Haas moved, and Mr. Leitch seconded to vacate a 15.15-foot-wide portion of the Shields Avenue right-of-way and a portion of the Electric Boulevard right-of-way to property owned by Avon Lake Regional Water. Motion carried (7-0).

### **GENERAL PUBLIC COMMENTS**

Ms. Debra Beal (resident) raised concerns during public comment about pedestrian safety at the intersection of Miller Road and Lake Road, particularly the lack of flashing lights or enhanced crosswalk features near the park. She also inquired about the demolition timeline of the Avon Lake Power Plant and whether the high-voltage power lines and the overpass/bridge would be removed.

Mayor Spaetzel stated that the City is aware of pedestrian safety concerns at this intersection. The Ohio Department of Transportation (ODOT) previously removed a traffic signal there, which limited options for installation. The City is currently working on a sidewalk improvement project in the area, sponsored by ODOT. Due to the timing and coordination with the state-funded project, the City cannot install flashing lights immediately but is considering improvements as part of the larger sidewalk project. The demolition of the power plant is progressing on schedule, with the main plant structure expected to be fully demolished by August 2025. The high-tension transmission lines and substations on the south side of the property are owned by FirstEnergy and will not be part of the demolition; they will remain in place. The overpass or bridge on the site is owned by Avon Lake Regional Water and will remain as well.

William Zimmerman (resident) praised the Planning Commission's decision to allow public comment before votes.

### **ADJOURNMENT**

Mr. Leitch moved, and Mayor Spaetzel seconded to adjourn the meeting at 8:40 pm. Motion carried (7-0).

---

Planning Commission  
Chairperson Ma

---

Recording Secretary  
Kelly La Rosa



## Harbor Crest Townhouse Subdivision Preliminary Plat Alternatives

# Supplemental Report

**To:** Avon Lake Planning Commission

**From:** Kelly La Rosa, Planning and Zoning Manager

**Date:** July 30, 2025

**Re:** Case No. CPC-25-9 Harbor Crest Townhouse Subdivision Preliminary Plat Alternatives

---

### Background

The Harbor Crest Townhouse Subdivision is a proposed 73-unit residential development by Pulte Homes, located on the east side of Avon Belden Road (SR 83), just south of Walker Road. The undeveloped site is adjacent to existing neighborhoods and commercial properties.

The applicant, represented by Jim Sayler of Henry G. Reitz Engineering Co., submitted a Preliminary Plat for Planning Commission review. During its July 1, 2025, meeting, the Commission expressed general support for the layout but requested alternative designs for two key elements:

- The ingress and egress on Avon Belden Road (SR 83), and
- The stub street connection at Audry Lane to the south.

This supplemental report presents those alternatives and includes updated input from safety forces, public works, and administrative staff to support the Planning Commission's final decision. These alternatives were not part of the original Development Review Committee (DRC) review, which evaluated only the initial configuration. Therefore, the departmental feedback in this report focuses specifically on the revised layouts and their potential impact on traffic flow, emergency access, and long-term planning goals.

### Design Alternatives and DRC Comments

#### I. Avon Belden Road (SR 83) Ingress/Egress Alternatives

The original preliminary plat proposed a divided boulevard-style entrance on Avon Belden Road, slightly offset from the Huntington Bank driveway. This design allowed for a wide, deep retention



basin that was both aesthetically pleasing and easy to maintain. In response to the Planning Commission's request to explore better alignment with the Huntington Bank entrance, Mr. Saylor presented two alternatives:

- Entry Alternate "1": Re-aligns the entry drive directly across from Huntington Bank and maintains one outbound lane.
- Entry Alternate "2": Similar to Alternate "1", but adds a dedicated left-turn lane, splitting outbound traffic movements.

These alternatives compress the stormwater basin, resulting in a reduction of the setback between the basin and the rear yards of the townhouses—from approximately 10 feet to 6 feet. Additionally, the basin's eastern portion is designed with a shallower profile, which may pose maintenance challenges over time, including increased potential for sediment accumulation and weed intrusion. While the overall layout remains functional, these changes may also impact emergency access along the rear of the units.

The following section summarizes the input provided by safety forces and city staff on the proposed ingress and egress alternatives.

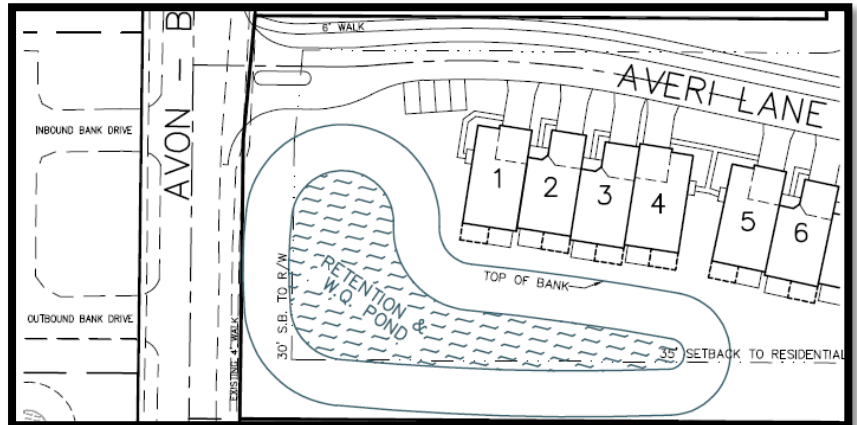


Figure 1: Excerpt Entry Original Layout Drawing

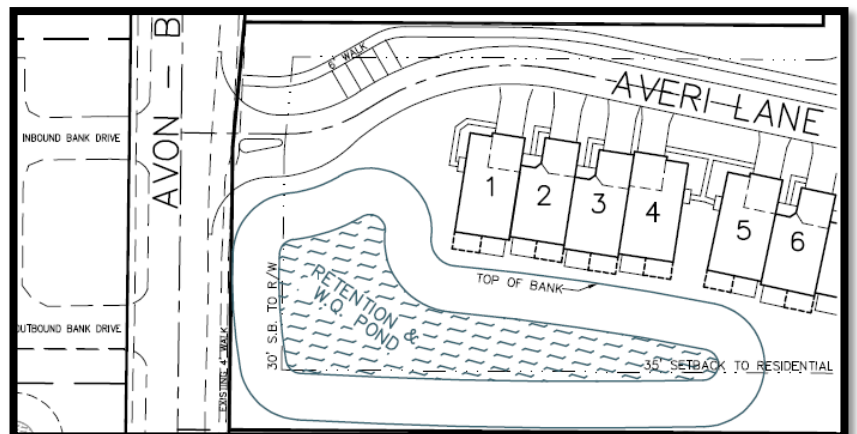


Figure 2: Excerpt Entry Alt 1 Drawing

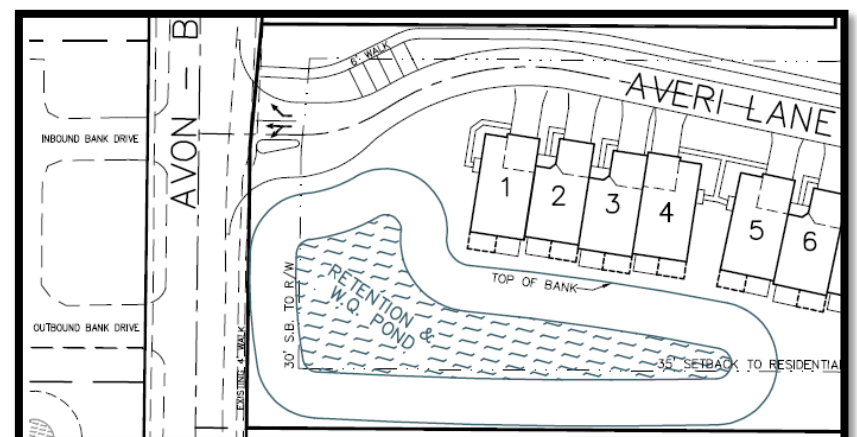


Figure 3: Excerpt Entry Alt 2 Drawing



Planning Commission  
Case No. CPC-25-9  
Harbor Crest Townhouse Subdivision  
Preliminary Plat Alternatives  
July 30, 2025  
Page 3 of 6

### **Fire Department**

Fire Chief Betsa expressed concerns regarding the proposed alignment of the subdivision's entrance with the Huntington Bank driveway across Avon Belden Road. He indicated a preference for the original offset design, citing potential traffic safety risks associated with aligning two driveways at an uncontrolled intersection. Chief Betsa noted that if the bank's traffic pattern were to change in the future, such as allowing two-way access, the alignment could lead to dangerous, conflicting turning movements. He also opposed the inclusion of a center island at the entrance, stating it could obstruct emergency vehicle access, particularly for units approaching from the north. Additionally, he raised operational concerns about the reduced setback between the retention basin and the rear of the northern units, warning that the narrower space could limit fire personnel access or hose deployment during an emergency.

### **Police Department**

Police Chief Robinson supported the traffic engineers' recommendation to align the development's entrance and exit with the bank's entry-only drive, expressing confidence in the technical analysis and its conclusion that this alignment would reduce overall traffic conflicts.

### **Public Works Department**

Public Works Director Liskovec acknowledged the preference for aligning the driveway with the Huntington Bank entrance, as supported by the traffic study and past city practices. He noted that while the addition of a left-turn lane in Alternate "2" improves traffic flow, it offers limited vehicle storage—likely accommodating only two cars—which could result in queued vehicles blocking right-turn movements. Director Liskovec also expressed concern over the revised stormwater basin design, explaining that the narrower and shallower configuration, particularly along the eastern side, may pose long-term maintenance challenges due to increased sedimentation and vegetation growth. Although he did not express a strong preference between the alignment options, he emphasized the trade-offs involved, particularly for stormwater management, vehicle circulation, and emergency access.

### **Mayor**

Mayor Spaetzel highlighted that aligning the subdivision's entrance with the Huntington Bank driveway supports both the traffic study's recommendations and the city's broader plan for coordinated access. He explained that this strategy is based on established traffic planning principles, but recognized Chief Betsa's concerns about possible turning conflicts at an uncontrolled intersection. Citing issues observed with offset driveways elsewhere in the city, Mayor Spaetzel emphasized the importance of carefully considering the traffic engineer's findings alongside departmental input to find a balance between traffic flow and public safety.

### **Zoning**

Planning and Zoning Manager Austin Page expressed a preference for Entry Alt #2".



## II. Southeastern Stub Street at Audry Lane Alternatives

The southeastern portion of the subdivision includes a proposed stub street that would serve three units and potentially connect to undeveloped land to the south, currently owned by Mr. Hubert. The original plan features a 22-foot-long roadway extending to the southern property line to preserve the option for future interconnectivity. In response to concerns raised by the Planning Commission regarding potential neighborhood opposition to a future connection, two design alternatives were developed:

- Stub Alternate “A”: reduces the length of the stub street from 22 feet to 18 feet and adds a landscaped mound at the end, reducing the number of guest parking spaces from three to two.
- Stub Alternate “B”: introduces a bend in the stub street and uses angled parking to retain three guest spaces while visually discouraging future extension.

Although both alternatives are intended to soften the appearance of future roadway connections, they do not preclude it. A permanent prohibition would require a legal mechanism such as a deed restriction or recorded easement.

The proposed stub street layout and its potential for future interconnectivity were reviewed and discussed among members of DRC. While the visual alternatives aim to address immediate concerns about neighborhood impact, city staff emphasized the importance of long-term planning considerations, particularly regarding emergency access, street length, and the legal implications of future connections. The following section summarizes the input provided by safety forces and city staff on the stub street configuration.

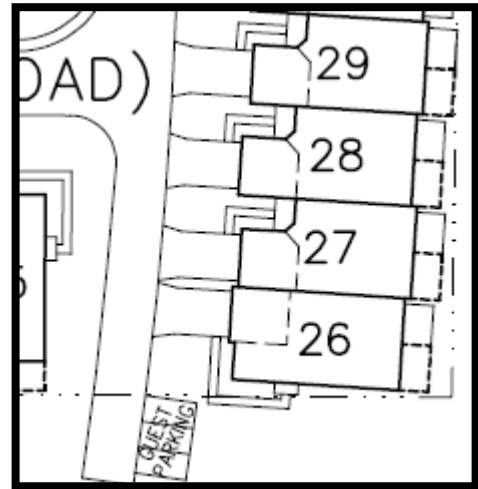


Figure 4: Excerpt Original Stub Layout

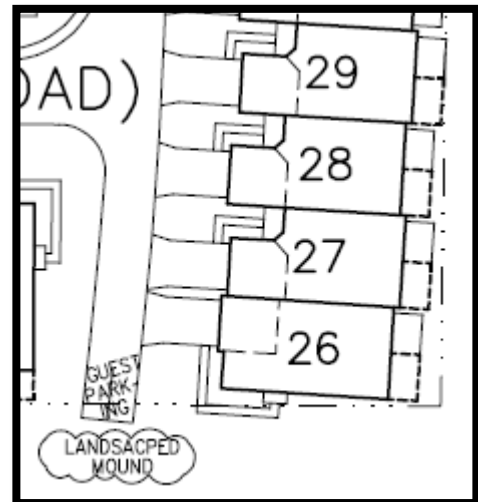


Figure 5: Excerpt Stub Alt "A"

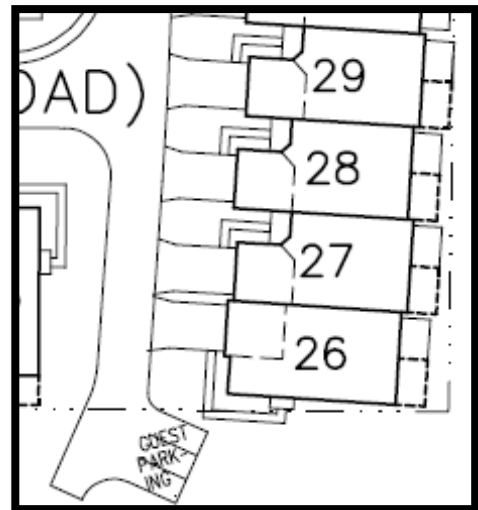


Figure 6: Excerpt Stub Alt "B"



Planning Commission  
Case No. CPC-25-9  
Harbor Crest Townhouse Subdivision  
Preliminary Plat Alternatives  
July 30, 2025  
Page 5 of 6

### **Fire Department**

Chief Betsa supports maintaining the potential for future connectivity at the southeastern stub street, emphasizing its importance for emergency response access, even if the land nearby remains undeveloped for years. He recommends keeping the full 22-foot length of the stub to ensure safe passage for fire trucks and suggests adding a turnaround or widening the end to improve maneuverability and effectiveness during emergencies.

### **Police Department**

Chief Robinson shares similar concerns about emergency access. He points out the risk of overflow parking on the street—especially during large events—that could block emergency vehicles, particularly if the street is narrow. He emphasizes the need for sufficient guest parking to reduce this risk and notes that enforcing parking rules on private streets is essential to maintaining clear access for safety personnel.

### **Public Works**

Director Liskovec supports keeping the stub street's connectivity potential, aligning with the city's broader planning. He recommends maintaining the 22-foot pavement, noting that an earlier plan to reduce it to 18 feet would not support vehicle circulation or access effectively. Director Liskovec warns that visual changes—like landscaped mounds or curves—could discourage future connections, but they do not legally prohibit them.

### **Mayor**

Mayor Spaetzel recognizes that some residents might oppose future connections but stresses that the Planning Commission must balance those concerns with the city's longstanding goal of fostering interconnectivity between developments. He points out that such connections have historically improved circulation and access, benefiting both routine traffic and emergency response.

### **Zoning**

Manager Page expressed a preference for Stub Alt "B".

### **Planning Considerations and Conclusion**

The Planning Commission is tasked with evaluating two key aspects of the Harbor Crest Preliminary Plat: the ingress/egress on Avon Belden Road (SR 83) and the stub street at Audry Lane. Each includes alternatives developed in response to previous feedback and departmental insights.

For the Avon Belden Road entrance, the original design includes a boulevard-style offset entry that supports a large stormwater basin with plenty of access for maintenance. Alternatives 1 and 2 align the entrance with the Huntington Bank driveway, consistent with the city's coordinated access goals. However, these options reduce the size of the retention basin, decreasing rear setbacks for the nearby townhouses and creating a shallower profile on the east side. While they



Planning Commission  
Case No. CPC-25-9  
Harbor Crest Townhouse Subdivision  
Preliminary Plat Alternatives  
July 30, 2025  
Page 6 of 6

retain the basic functionality, these changes could complicate long-term stormwater maintenance and emergency access.

For the Audry Lane stub street, the original plan preserves a 22-foot connection to the southern property line and includes three guest parking spaces, maintaining the potential for future interconnectivity. Stub Alternate A reduces the pavement length, adds a landscaped mound, and provides two guest spaces. Alternate B introduces a curved layout with angled parking to retain three guest spaces while visually discouraging future extension. While both alternatives soften the appearance of a connection, they do not legally prevent it. A deed restriction or recorded easement could permanently prevent extension.

In evaluating these options, the Planning Commission should consider:

- Emergency access and overall vehicle circulation;
- Maintenance challenges tied to any modified site geometry;
- The limited enforceability of visual-only design features;
- The city's long-standing policy of encouraging interconnectivity between adjacent developments.

While the applicant has not formally indicated a preference, the original design likely reflects their intent. The alternatives were developed in response to the Planning Commission's direction and are presented to support a balanced, well-informed decision

### **Attachments**

- James Saylor's Letter dated July 7, 2025, includes site layout options for both ingress/egress and stub street scenarios.

# THE HENRY G. REITZ ENGINEERING CO.

*Civil Engineers and Surveyors*

James T. Sayler, P.E., P.S., *President*  
Linda S. Rerko, *Sec.-Treas.*

4214 ROCKY RIVER DRIVE  
CLEVELAND, OHIO 44135-1948

TELEPHONE: (216) 251-3033  
EMAIL: reitz@reitzeng.com

July 7, 2025

Ms. Kelly La Rosa  
Avon Lake Planning & Zoning Manager  
150 Avon Belden Road  
Avon Lake, OH 44012

Re: Harbor Crest Townhouse Subdivision Preliminary Plat

Dear Ms. LaRosa:

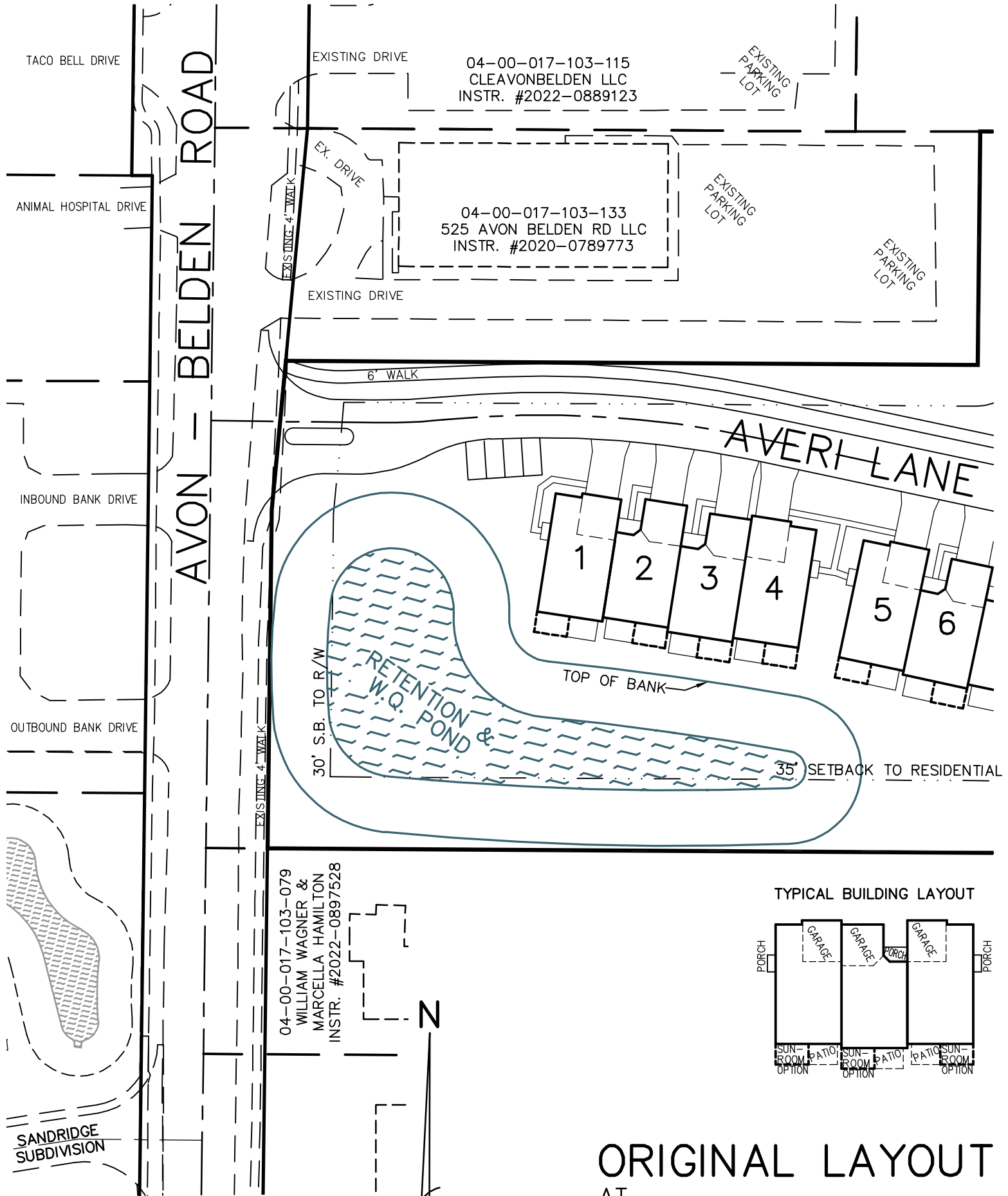
In response to the request of Planning Commission and your letter of July 2, 2025, please find the 6 attached drawings for review and comments at the next available D.R.C. meeting:

- a. Original Layout detail of intersection at Avon Belden Road
- b. Entry Alt. #1 shows a revised alignment for the intersection at Avon Belden Road where the exit from the project has been moved to be opposite the Huntington Bank inbound drive.
- c. Entry Alt. #2 shows a revised alignment for the intersection at Avon Belden Road where the exit from the project has been widened to provide a left turn lane which in a location opposite the Huntington Bank inbound drive.
- d. Original Layout detail of Southerly stub of Audry Lane.
- e. Stub Alt. "A" shows a revised method of terminating the Southerly stub with 2 guest parking spaces moved and a landscaped mound added to augment Planning Commission's proposal to eliminate the possibility of connecting to any future development to the South.
- f. Stub Alt. "B" shows a revised method of terminating the Southerly stub with 3 guest parking spaces moved and the street alignment angled to augment Planning Commission's proposal to eliminate the possibility of connecting to any future development to the South.

Either of the two entry alternatives would require the reshaping of the proposed retention basin as shown in order to provide the same retention volume. If it is the Planning Commission's preference to restrict against any future connection to the South, it could be accomplished either by a restriction in the homeowner association covenants and restrictions or as a deed restriction added when the land transfers to the homeowner association. The developer requests that D.R.C. provide feedback so that a motion can be considered at the August 5, 2025 Planning Commission meeting to approve either the original layout or any one or two of the above alternate layouts.

Very truly yours,  
THE HENRY G. REITZ ENGINEERING CO.

By  
James T. Sayler, President



TACO BELL DRIVE

EXISTING DRIVE

04-00-017-103-115  
CLEAVONBELDEN LLC  
INSTR. #2022-0889123

EXISTING  
PARKING  
LOT

ANIMAL HOSPITAL DRIVE

AVON - BELDEN ROAD

EXISTING 4' WALK

EX. DRIVE

04-00-017-103-133  
525 AVON BELDEN RD LLC  
INSTR. #2020-0789773

EXISTING  
PARKING  
LOT

EXISTING  
PARKING  
LOT

EXISTING DRIVE

6' WALK

INBOUND BANK DRIVE

AVERI LANE

1

2

3

4

5

6

OUTBOUND BANK DRIVE

30' S.B. TO R/W

RETENTION POND &  
W.Q.

TOP OF BANK

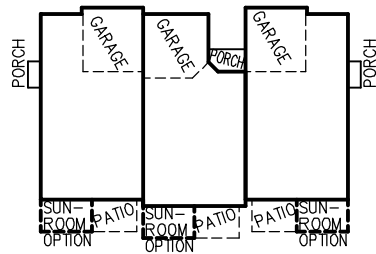
35' SETBACK TO RESIDENTIAL

04-00-017-103-079  
WILLIAM WAGNER &  
MARCELLA HAMILTON  
INSTR. #2022-0897528

N

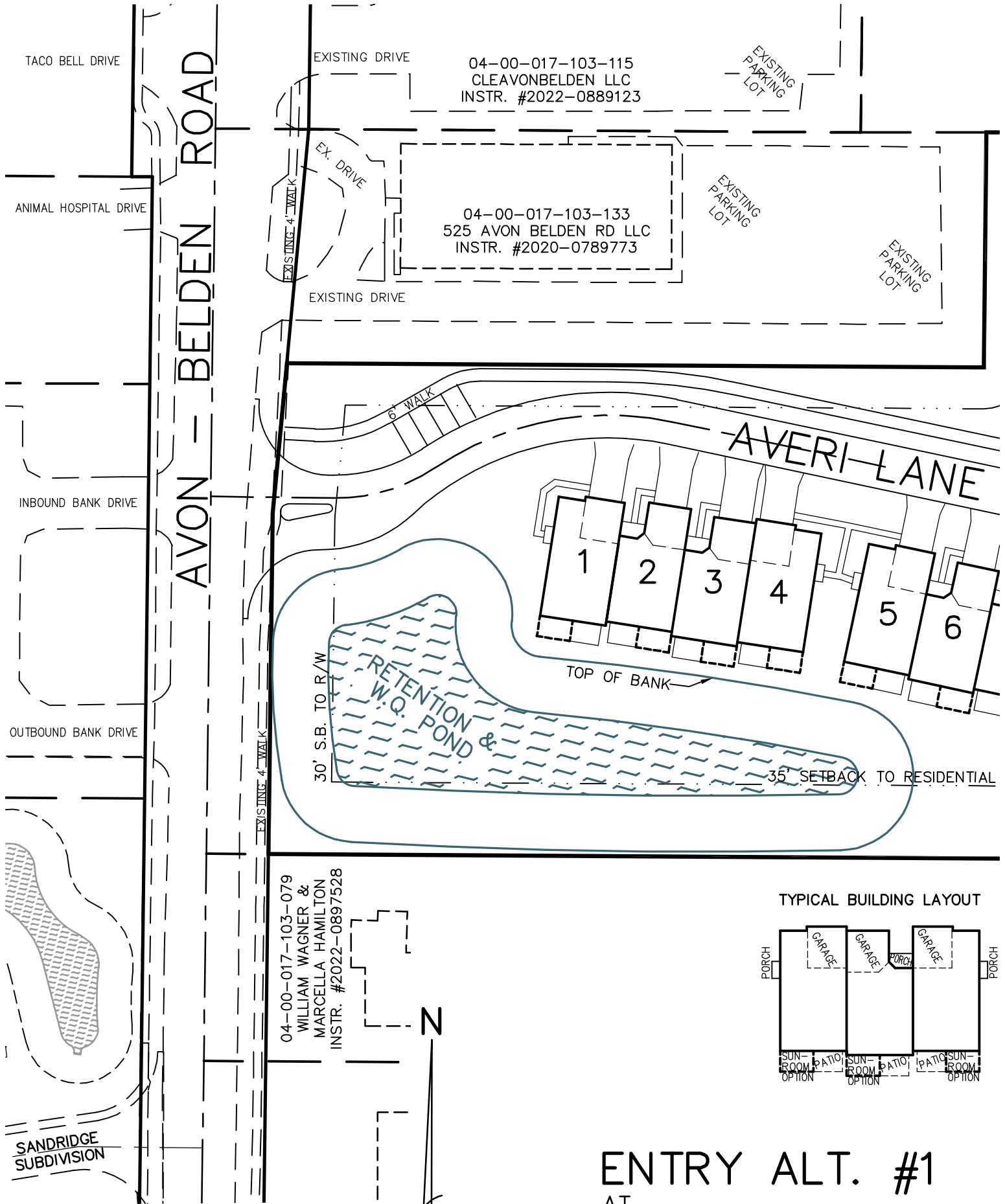
SANDRIDGE  
SUBDIVISION

TYPICAL BUILDING LAYOUT

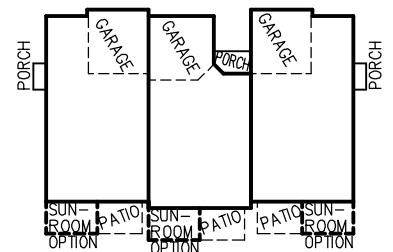


GRAPHIC SCALE  
0 30 60  
REITZ ENGINEERING CO.

ORIGINAL LAYOUT  
AT  
HARBOR CREST SUBD.  
FOR  
PULTE HOMES OF OHIO



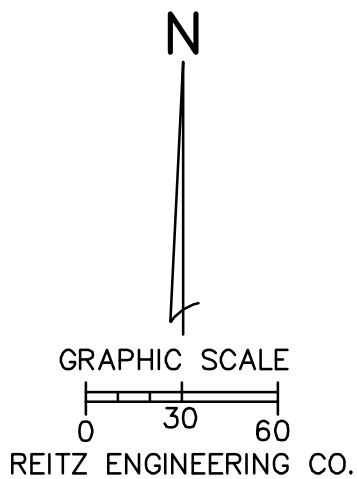
TYPICAL BUILDING LAYOUT



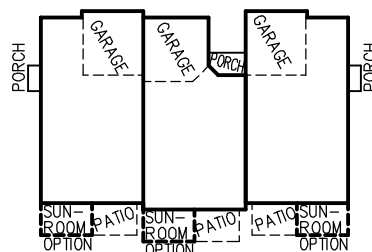




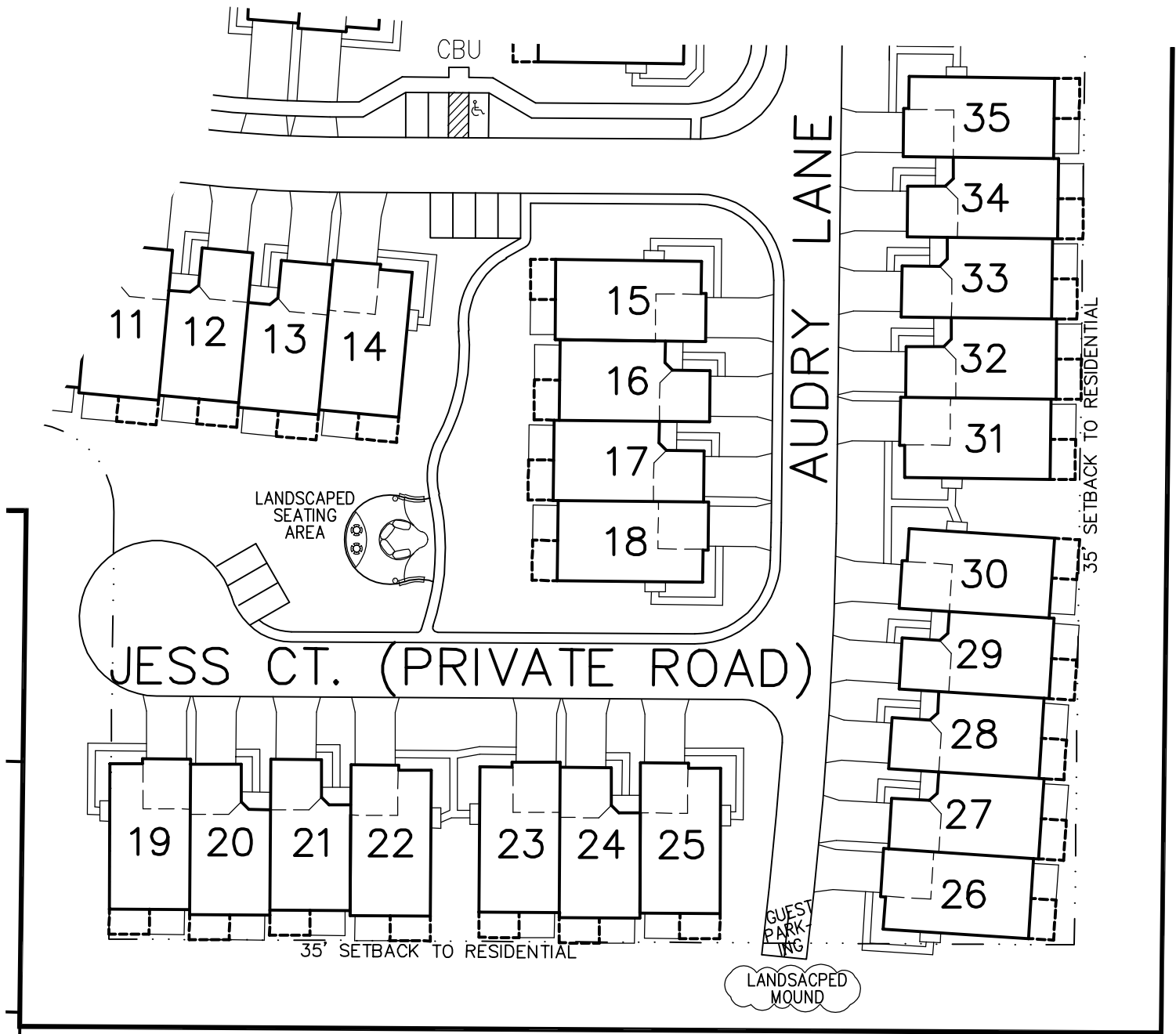
04-00-017-103-346  
 WILLIAM D. HUBERT  
 INSTR. #2016-0605682



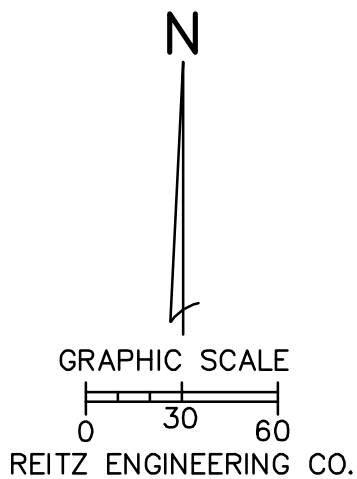
TYPICAL BUILDING LAYOUT



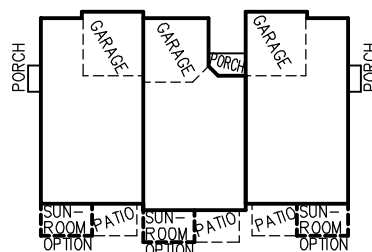
ORIGINAL LAYOUT  
 AT  
 HARBOR CREST SUBD.  
 FOR  
 PULTE HOMES OF OHIO



04-00-017-103-346  
 WILLIAM D. HUBERT  
 INSTR. #2016-0605682



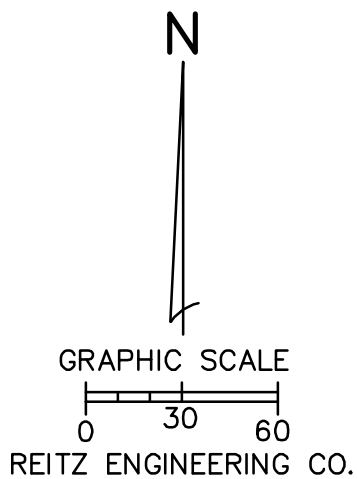
TYPICAL BUILDING LAYOUT



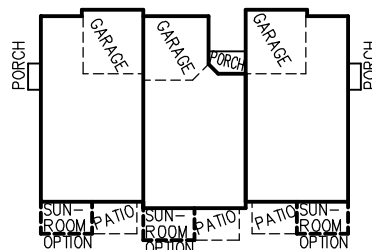
STUB ALT. "A"  
 AT  
 HARBOR CREST SUBD.  
 FOR  
 PULTE HOMES OF OHIO



04-00-017-103-346  
 WILLIAM D. HUBERT  
 INSTR. #2016-0605682



TYPICAL BUILDING LAYOUT



STUB ALT. "B"  
 AT  
 HARBOR CREST SUBD.  
 FOR  
 PULTE HOMES OF OHIO



## HARBOR CREST TOWNHOUSE SUBDIVISION – PRELIMINARY PLAT

# Report

**To:** Planning Commission

**From:** Kelly La Rosa, Planning and Zoning Manager

**Date:** June 26, 2025

**Re:** Case No. CPC-25-9, Pulte Homes of Ohio, LLC, Major Subdivision Preliminary Plat Approval for Harbor Crest Townhouse Subdivision, located in the southeast corner of Walker Road and Avon-Belden Road (SR 83).

### PROJECT OVERVIEW

Harbor Crest Townhouse Subdivision is a proposed residential development by Pulte Homes of Ohio, consisting of 73 townhouse units on individually owned (fee simple) footprint parcels with access provided by newly constructed private streets. The site was rezoned to R-3 Residential with a density restriction limiting the project to 73 units, upon recommendation from the Planning Commission on April 1, 2025 to the City Council.

### PROJECT DESCRIPTION

**Owner:** Pulte Homes of Ohio, LLC, 387 Medina Road, Medina

**Applicant/Engineer:** Henry G. Rietz Engineering Co., 4214 Rocky River Dr., Cleveland



Figure 1: Maps Data: Google Earth 6/2/2015



Planning Commission  
Case No. CPC-25-9  
Harbor Crest Townhouse  
Preliminary Plat  
June 26, 2025  
Page 2 of 5

**Location:** The property is located at the southeast corner of Walker Road and Avon-Belden Road (State Route 83).

**Approvals:** The site was rezoned to R-3 Multi-Family Residential by the Planning Commission on April 1, 2025.

**Current Zoning:** R-3 Multi-Family Residential

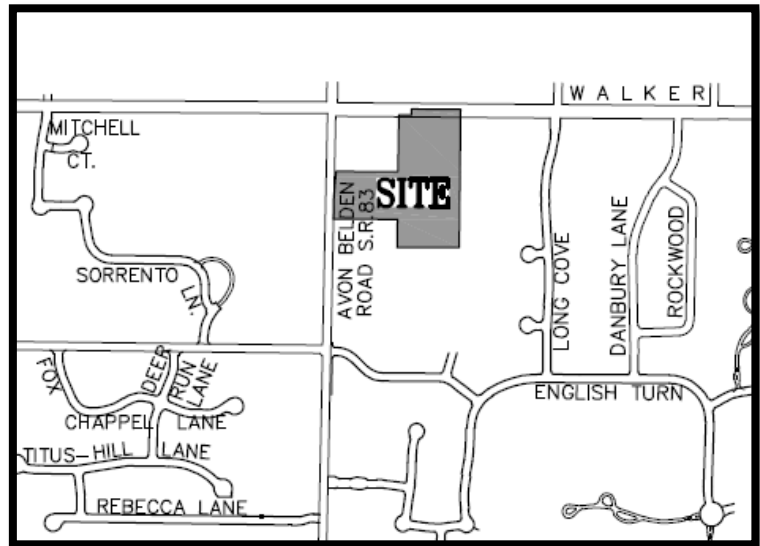


Figure 2: Vicinity Map

#### Surrounding Context:

The site is bordered by commercial properties to the north and west (B-2), with low-density residential areas to the south and east (R-1A). The Comprehensive Land Use Plan (CLUP) designates it as a prime location for mixed-use and higher-density residential development, encouraging walkability and connectivity with nearby commercial areas.

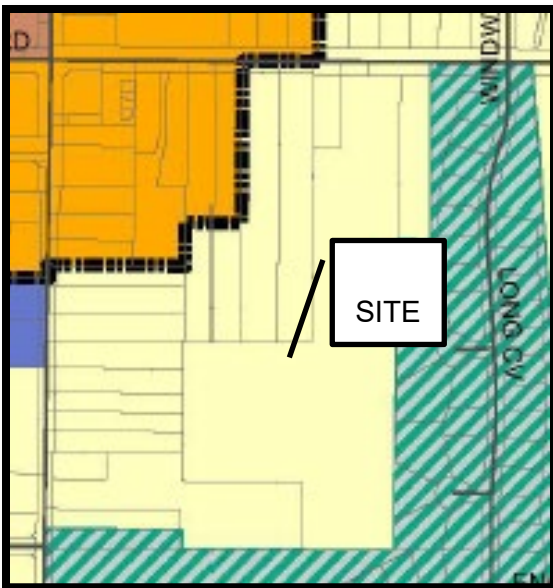


Figure 3: Excerpt from Avon Lake Zoning Map

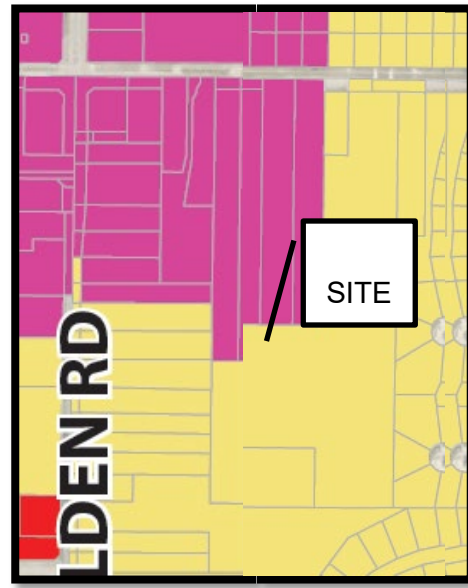


Figure 4: Excerpt from CLUP



Planning Commission  
Case No. CPC-25-9  
Harbor Crest Townhouse  
Preliminary Plat  
June 26, 2025  
Page 3 of 5

## **APPLICABLE CODE SECTIONS**

### **Section 1214.05: Major Subdivision**

This section governs the review of major subdivisions, ensuring consistency with zoning regulations, thoroughfare planning, infrastructure requirements, and integration with existing documents.

### **Section 1238: Subdivision Design Standards**

This section defines Subdivision Design Standards, including lot layout, block length, right-of-way configuration, utilities, sidewalks, landscaping, and connectivity.

## **PROJECT ANALYSIS**

Pulte Homes of Ohio proposes the development of Harbor Crest Townhouse Subdivision, a residential community consisting of 73 fee-simple townhome units located at the southeast corner of Walker Road and Avon-Belden Road (SR 83). The approximately 11.86-acre site was rezoned to R-3 Multi-Family Residential on April 1, 2025, with a specific density restriction limiting the development to no more than 73 units.

The proposed preliminary plat includes a network of private streets that provide internal circulation and access to all units. Each townhome will be constructed on individual parcels, with standard driveway lengths of 25 feet and consistent 22-foot street widths. The site has been carefully configured in response to feedback received during the rezoning process, resulting in improved vehicular access, pedestrian accommodations, aesthetic enhancements along Avon-Belden Road, and the inclusion of common open space.

## **SUSTAINABILITY CONSIDERATIONS**

The Harbor Crest Townhome development has been designed with a strong emphasis on sustainability and responsible stormwater management. Following Avon Lake's stormwater and water quality regulations, the project includes a comprehensive system of retention basins to reduce flooding and minimize erosion. These basins are not only functional but also serve as visual and environmental amenities within the site design. In addition to stormwater retention, the development promotes groundwater replenishment through enhanced infiltration practices.



Planning Commission  
Case No. CPC-25-9  
Harbor Crest Townhouse  
Preliminary Plat  
June 26, 2025  
Page 4 of 5

Additionally, construction-phase sediment and waste control measures will be implemented to protect surrounding ecosystems and limit off-site impacts. Post-construction, the project will feature water quality basins designed to capture and treat runoff, ensuring long-term environmental performance. These measures demonstrate the applicant's commitment to sustainable development practices that support ecosystem health, improve water quality, and contribute to the resilience of the built environment.

## **DEVELOPMENT REVIEW COMMITTEE**

The Development Review Committee comprises representatives from Community Development, Engineering, Building, Public Works, Avon Lake Regional Water, Fire, and Police Departments. All responses received to date are included in the attachments. The preliminary Plat has been updated to reflect comments from the Community Development Department.

## **REVIEW AND RECOMMENDATION BY THE COMMISSION**

To approve a major subdivision, the Commission must determine that the Preliminary Plat meets all applicable standards as outlined in Section 1214.05 of the Avon Lake Planning and Zoning Code. The following criteria shall be considered:

- That the major subdivision complies with all applicable provisions of this code;
- That the major subdivision does not conflict with other regulations, plans, or policies of the City;
- That it is designed to be harmonious with the existing immediate or surrounding area or in keeping with the intended character of such area;
- That it follows the Master Thoroughfare Plan and minimizes traffic congestion in public streets;
- That it will not adversely affect the delivery of governmental services;
- That comments from review agencies have been adequately considered and addressed
- That it conforms to the approved preliminary Plat if submitted and approved.

After reviewing the Preliminary Plat, the Commission shall determine whether to return it to the applicant for revisions or to forward a recommendation to the City Council for approval, approval with conditions, or denial. If recommending denial, the Commission must identify the specific review criteria that were not met and provide the reasoning behind that conclusion.



Planning Commission  
Case No. CPC-25-9  
Harbor Crest Townhouse  
Preliminary Plat  
June 26, 2025  
Page 5 of 5

**Potential Motions:** The Commission must make a motion in the affirmative. At least four affirmative (“yes”) votes are required to recommend approval, or four negative (“no”) votes are required to recommend denial to the City Council. The sample motion provided below is for guidance only and does not imply a recommendation or predetermined outcome.

I move to recommend to the City Council that Case No. CPC-25-9, submitted by Pulte Homes of Ohio LLC, Major Subdivision Preliminary Plat for Harbor Crest Townhouse, be approved, finding that the proposed subdivision complies with applicable zoning and subdivision regulations and serves the public interest.

Or subject to the following conditions:

---

---

---

---

## **SUBSEQUENT ACTION**

Upon receiving approval from the Planning Commission, the Preliminary Plat will be forwarded to the City Council for final review and action. If the Planning Commission recommends denial, the applicant may not proceed with the related components of the subdivision until recommendations for approval are issued.

## **ATTACHMENTS**

- Planning Commission Application
- DRC Comments
- Applicant’s response
- TIS Review

## CPC-25-9

### Planning Commission Application

Status: Active

Submitted On: 5/29/2025


### Primary Location

32701 WALKER RD  
AVON LAKE, OH 44012

### Owner

WOODCRAFT PROPERTIES LTD  
32741 WALKER RD AVON LAKE, OH 44012

### Applicant

 James Sayler  
 216-251-3033  
 reitz@reitzeng.com  
 4214 Rocky River Dr.  
Cleveland, OH 44135

## Property or Parcel Information

### Zoning Classification

R-3

### Present Use\*

Vacant

### Type of Request\*

Major Subdivision - Preliminary Plat

### Preliminary



### Final



### General Description of Project\*

Preliminary Plat for 73 unit townhouse developement on recently rezoned land. A lot split and consolidation survey and legal descriptions to create single ownership for land to be developed is currently under review at the City and will be filed at Lorain County prior to development.

### Have you had your meeting with the Development Review Committee?\*

No

**Your application will not be reviewed until you have met with the Development Review Committee (DRC).  
Please submit the DRC application before submitting this application.**

## Applicant Information

**Applicant** is the Property Owner or Property Owner's Designee.

**Project Manager** will be the person working closest with the plans and will be the main point of contact for the Planning Department's questions.

### Applicant Role\*

Subdivider

### Applicant Name\*

Reitz Engineering Co.

**Address\***

4214 Rocky River Dr.

**City\***

Cleveland

**State\***

OH

**Zip\***

44135

**Phone\***

216-251-3033

**Email\***

jts@reitzeng.com

**Project Manager**

Pulte Homes, Keith Filipkowski

**Project Manager Phone**

261-308-1627

**Project Manager Email**

Keith.Filipkowski@Pulte.com

---

## Property Owner Information

**Name\***

Various entities assoc. with Kopf Builders

**Address\***

420 Avon Belden Rd.

**City\***

Avon Lake

**State\***

OH

**Zip\***

44012

**Phone\***

440-933-6908

---

## Signature

**Applicant Signature\***

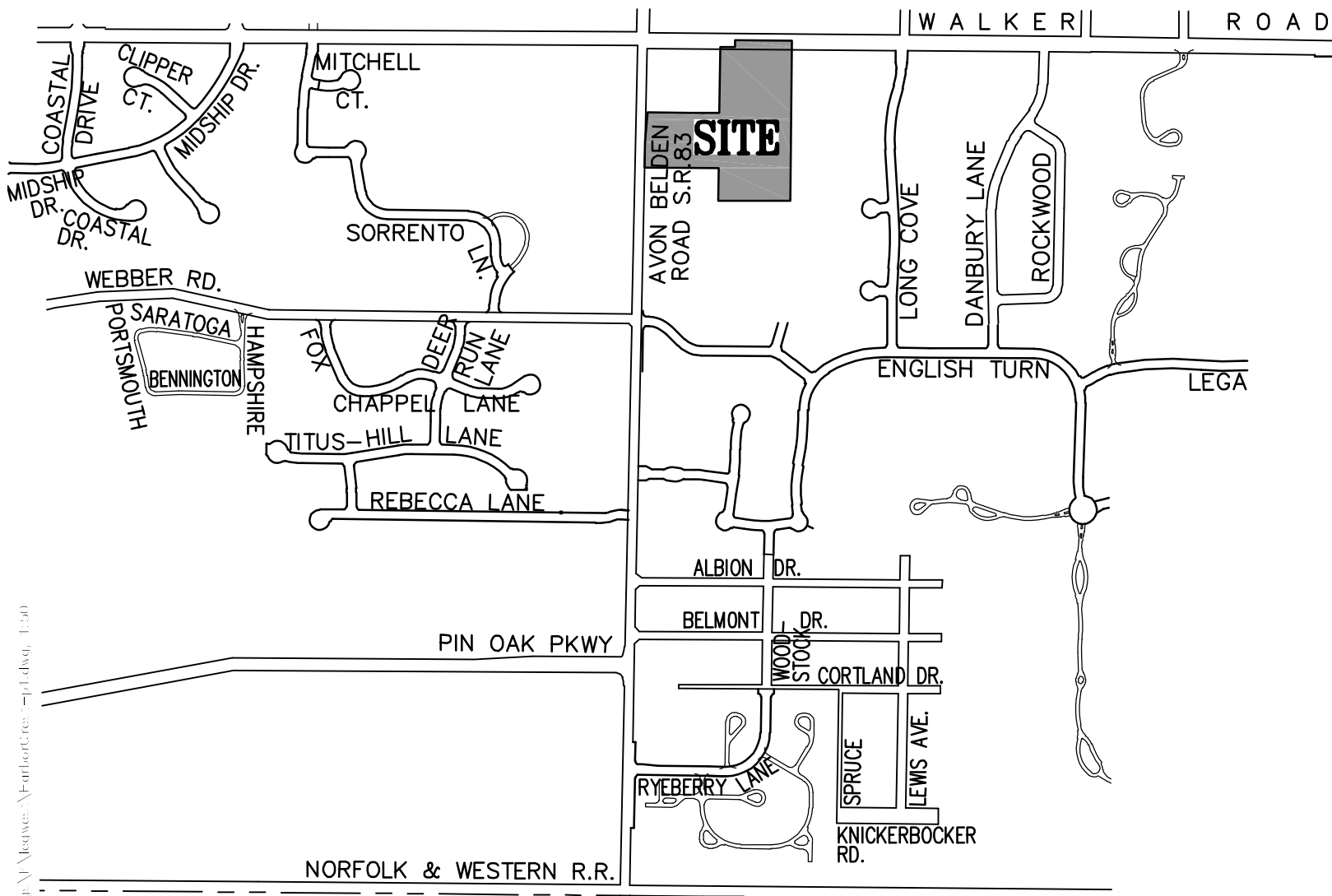


James Sayler

May 28, 2025

# HARBOR CREST SUBDIVISION VICINITY MAP

BEING PART OF AVON TOWNSHIP SECTION NO. 17 NOW IN THE  
CITY OF AVON LAKE, COUNTY OF LORAIN, STATE OF OHIO



# Harbor Crest Townhomes

Applicant: Pulte Homes of Ohio

Narrative update: May 28, 2025

The project is proposed to consist of 73 townhouse units on fee simple footprint parcels abutting new private streets. The project land has been granted a rezoning to R-3 with a density restriction to allow no more than the 73 units proposed.

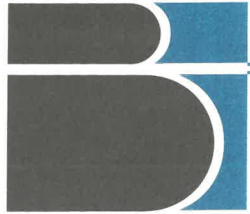
The project layout follows the concept plan that accompanied the rezoning request that was approved at Planning Commission on April 1, 2025. It maintains the following features that were included in response to comments that had been received prior to the rezoning meeting:

- 1) The project limits were revised to eliminate a small, isolated area of land that was unsuitable for development and would have been difficult to access for any recreational uses.
- 2) The Avon-Belden Road entry was moved North to avoid conflict with Bank exit lanes across the street which had been identified as a potential problem area in the traffic study.
- 3) The aesthetics of the Avon-Belden Rd. frontage were improved and the project's density was moved away from the main route into the city by:
  - a. Shifting the majority of the retention volume away from the Walker Rd. entry which resulted in a more aesthetic pond being proposed at Avon Belden Rd.
  - b. Moving the divided entry so the landscaped island will be provided along Avon Belden Rd instead of Walker Rd.
- 4) The Southeast portion of the project was realigned to provide:
  - a. A more logical location for a future emergency access point for any future development to the South along the private street stub.
  - b. A better turnaround option for public safety and delivery vehicles.
  - c. Better access for an area that can be utilized for a pocket park.
  - d. Increased stub street width and driveway lengths to be consistent with the 22' width and 25' length found on the primary private streets.
- 5) Pedestrian access was improved by:
  - a. Moving the walk for the main loop off the edge of pavement to allow for a better streetscape with 4' to 18' wide tree-lawns.
  - b. Making the main loop sidewalk 6' wide pavement to improve its usefulness for multipurpose active uses.

Additional details were added to the surrounding properties and to show the topography and location of the proposed utilities, and hardscape for a 'pocket park' with a landscaped seating area was added in the Southeast portion of the project.

The Henry G. Reitz Engineering Co.  
by: James Sayler, President

The project will be designed in accordance with Avon Lake storm water management and storm water quality standards to promote ecosystem health through flood and erosion reduction provided by a storm water retention basins, ground water replenishment provided through enhanced infiltration practices, sediment control and waste management strategies provided by construction site storm water management controls and post-construction water quality enhancements provided by water quality features included in the design of the water quality basins.



**BRAMHALL**  
ENGINEERING &  
SURVEYING COMPANY

801 Moore Road • Avon, Ohio 44011 • 440/934-7878 • Fax 440/934-7879

*Equal Opportunity • Affirmative Action Employer*

June 23, 2025

Ted Esborn  
Community Development Director  
City of Avon Lake  
150 Avon Belden Road  
Avon Lake, Ohio 44012

Reference: Preliminary Plat  
Harbor Crest Townhouse Subdivision

Mr. Esborn:

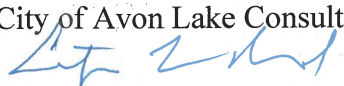
On behalf of the City of Avon Lake we have reviewed the Preliminary Plat submitted for Harbor Crest Townhouse Subdivision, as prepared by the Henry G. Reitz Engineering Company, and offer the following comments:

1. The TIS review, performed by Transystems dated April 22, 2025, recommended that consideration be given to realigning the Averi Lane access to SR 83 with the existing Huntington Bank entrance drive on the west side of SR 83.
2. Comments on the Proposed Lot Split/Consolidation have been provided.
3. The Applicant will need to provide Autoturn Exhibits for Fire Trucks at the Avon Belden Road (SR83) and Walker Road access points.

If you have any questions, please call.

Sincerely,

BRAMHALL ENGINEERING & SURVEYING CO.  
City of Avon Lake Consulting Engineers

  
Christopher L. Howard, P.E., CPESC  
City Engineer

## Zoning Review - Austin

Record No. CPC-25-9

Status	Completed	Became Active	June 4, 2025
Assignee	Austin Page	Due Date	June 14, 2025


### Primary Location

32701 WALKER RD  
AVON LAKE, OH 44012

### Owner

WOODCRAFT PROPERTIES LTD  
32741 WALKER RD AVON LAKE, OH 44012

### Applicant

 James Sayler  
 216-251-3033  
 reitz@reitzeng.com  
 4214 Rocky River Dr.  
Cleveland, OH 44135

## Messages

Austin Page	June 17, 2025 at 2:01 pm
<p>1.In the R-3 district, the maximum building height is 45'. Please add info to the site table and/or notes.</p> <p>2.It is assumed but please add a note stating that the development HOA is responsible for the ownership and maintenance of the island on Averl Lane.</p> <p>3.The end units (1, 24 &amp; 73) will front on the private streets but have a secondary frontage-like appearance onto Avon Belden and Walker Road. The "sides" shall mimic a front facade to enhance the curb appeal and overall streetscape, rather than a big blank wall.</p> <p>4.What is the plan for screening alongside the adjacent residential zoned properties? Please provide a landscape/screening plan. Or identify screening locations and type on the preliminary plan.</p> <p>5.Please add a note to the plan about a potential future connection at the southern end of Audry Lane (obviously it is assumed). It has become common for property owners near current stub-outs and connections to question these stub-outs, connections, etc. so a note would be nice for further transparency and any future issues.</p> <p>6.The Averl Lane exit to Avon Belden Rd appears to be one lane. Any ideas to widen this for two lanes? One for left turn and one for the right. I understand the traffic study didn't call for any of these types of improvements, but I believe this could be beneficial based on the location. Huntington Bank, Taco Bell, Las Margaritas in the area have these two dedicated lanes. Totally understand the level of traffic will not be like these uses. Not required but recommended.</p> <p>7.The CBU is shown off the street, with designated parking spaces. This helps to prevent vehicles from obstructing the roadway. Much appreciated.</p> <p>8.Please depict any development signage locations.</p> <p>9.Setback lines are depicted, and notes reference the distances. However, it would be beneficial to see the numerical value depicted (25', etc.) instead of having to look through the notes.</p>	
Austin Page	June 25, 2025 at 11:03 am
<p>Per the response letter and plans dated 6/20, all of my comments have been addressed.</p> <p>Thank you.</p>	
Austin Page	June 25, 2025 at 11:15 am
<p>However, I have one new comment. Landscaping is shown out near the entrance/exit to Avon Belden Road. There are "small ornamental trees" depicted and want to make sure that these trees do not impede vehicle sight lines exiting the site looking south. Based on the "island" that is in place and where sidewalk and stop sign, it may be ok but please look at this to ensure there is no visual impairment caused by the landscaping.</p>	

## ALRW Review - Jarod L.

Record No. CPC-25-9

Status

Completed

Became Active

June 4, 2025

Assignee

Jarod Larson

Due Date

June 14, 2025

### Primary Location

32701 WALKER RD  
AVON LAKE, OH 44012


### Owner

WOODCRAFT PROPERTIES LTD  
32741 WALKER RD AVON LAKE, OH 44012

### Applicant

James Sayler

216-251-3033

reitz@reitzeng.com

4214 Rocky River Dr.  
Cleveland, OH 44135

## Messages

Jarod Larson

June 9, 2025 at 7:41 am

ALRW will need to perform a detailed engineering review of the improvement plans once they are available.

# THE HENRY G. REITZ ENGINEERING CO.

*Civil Engineers and Surveyors*

James T. Sayler, P.E., P.S., *President*  
Linda S. Rerko, *Sec.-Treas.*

4214 ROCKY RIVER DRIVE  
CLEVELAND, OHIO 44135-1948

TELEPHONE: (216) 251-3033  
EMAIL: reitz@reitzeng.com

June 20, 2025

Ms. Kelly LaRosa  
Avon Lake Planning & Zoning Manager  
150 Avon Belden Road  
Avon Lake, OH 44012

Re: Harbor Crest Townhouse Subdivision Preliminary Plat

Dear Ms. LaRosa:

The following revisions were made to the drawings in response to the Zoning Department review comments that we received. In addition, the lot numbering was revised in conjunction with a revision that splits the construction of the project into two phases. Twelve full sized hard copies will also be delivered to your office to be included in the packets being sent to planning commission members.

1. A line was added under "Notes" in the bottom center of the drawing that indicates that the typical building height is approximately 26'.
2. The seventh item under "Notes" in the bottom center of the drawing was edited to indicate that the HOA is responsible for the entry island maintenance.
3. A line was added under "Notes" in the bottom center of the drawing that indicates that the side walls of units 1, 43 & 73 are to have enhanced architectural details.
4. A landscape plan was prepared and will be uploaded to the OpenGov portal.
5. The developer is willing to consider making accommodations for extension of the stub street to the South after getting input from a discussion of the issue at Planning Commission.
6. The developer requests having only one exit lane from the project onto Avon Belden Road to minimize the visual impact of the amount of concrete at the main entry.
7. No revision required.
8. The main entry sign at Avon Belden Rd. and a secondary sign at Walker Rd. are shown in detail in the landscape plans and they were added to the preliminary plat.
9. The setback lines were labeled as requested.

Very truly yours,  
THE HENRY G. REITZ ENGINEERING CO.

By  
James T. Sayler, President

## MEMORANDUM

[www.transystems.com](http://www.transystems.com)**To:** Mayor Mark Spaetzel  
City of Avon Lake, Ohio**From:** Aaron Grilliot, PE, PTOE  
TranSystems**Date:** April 22, 2025**Subject:** Harbor Crest Development TIS Review

---

## Introduction

This memorandum addresses the review of the Proposed Harbor Crest Residential Development Traffic Impact Study (TIS) dated April 2, 2025. The review examined the study assumptions, methodology, and findings.

## Review Comments

The assumptions, methodology, and findings in the above-referenced study were generally found to follow standard practice, with the following items specifically noted:

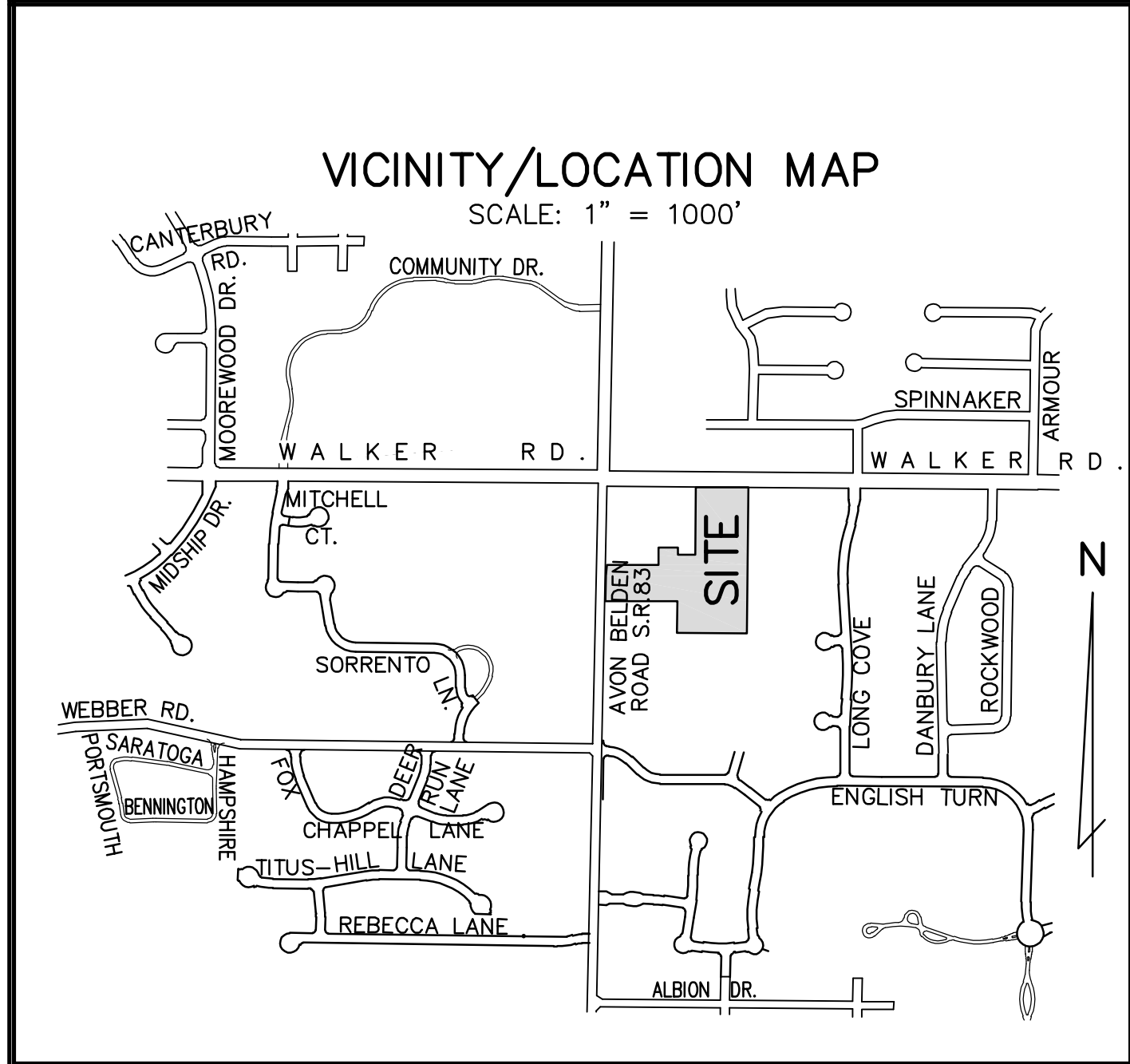
- While some minor adjustments to the geographic distribution of site traffic could be considered, the low volume of site-generated trips would not substantially alter the assignment of trips added to the background (No Build) traffic. The site is projected to generate less than one inbound and one outbound trip every two minutes on average during peak hours.
- The anticipated traffic volumes for the future Sandridge development (to the south) were added to the No Build condition, according to the narrative. However, no exhibits were provided which visually show the details of how these volumes were distributed, so the No Build volumes were not fully backchecked as part of the review. From the narrative description of the methodology, the No Build volumes appear to have been developed according to standard practice.
- The 0.5% background growth rate seems acceptable based on the TMFS report from ODOT's database for the area.
- Peak hours, DHV factors, and PHFs appear to be correct.
- Turn lane warrants were performed for the development entrances and no turn lanes appear to be warranted.
- The HCS calculation queue lengths do not show a need for extension of the existing dedicated turn lanes at the intersection of SR 83 and Walker Road. However, an Ohio Department of Transportation Location and Design Manual (ODOT L&D) turn lane calculation was not performed as part of the study. TranSystems performed an independent ODOT L&D calculation and determined that the existing NB LT turn lane does not meet the ODOT turn lane length recommendations. It was also determined that the existing dedicated EB LT, SB LT, and SB RT turn lanes meet the ODOT turn lane length recommendations but are not long enough to prevent the through lane queues from blocking entry into the turn lanes based on the anticipated through movement queue lengths according to

the ODOT calculations. However, in all instances, the turn lane length deficiencies are present in the No Build condition. The Build condition volumes did not increase the required turn lane lengths per ODOT criteria; therefore, these storage length deficiencies would not be the responsibility of the developer.

- The PM peak at the proposed development access drive on SR 83 shows LOS E for WB traffic exiting the proposed development. However, this impacts only the development and does not affect traffic operations on SR 83.
- The HCS calculations at SR 83 and Walker Road do not currently include pedestrian walk or clearance times. These timings should be incorporated into the signal capacity analysis as there are crosswalks on all four legs of the intersection. However, it is not anticipated that these refinements will materially impact the outcome of the study or the need for improvements at the SR 83 and Walker Road intersection.
- A previous version of the study, dated March 5, 2025, was referenced to better understand the changes made in the latest revision to the study. The original study showed a significantly different development layout than the updated study dated in April 2025. The original study showed the proposed SR 83 access point for the residential development across from the bank exit drive. In the updated study, this had been modified to show the proposed development access drive across from the bank entrance location. This change resulted in the elimination of the LOS issues seen in the original study for the bank drive in the PM peak, which was caused by opposing side street vehicles competing for gaps in north-south traffic on SR 83.
- The original study showed that the presence of the new development drive would increase delays for the eastbound exiting vehicles from the bank, as both eastbound and westbound vehicles would compete for the same gaps in northbound/southbound traffic flows. A review of the analysis in the revised study showed that queues in all directions at the proposed SR 83 access point opposite the bank entrance would not be expected to exceed one vehicle in length. Therefore, while no calculations were provided for the bank exit drive in the updated study, the operations for the bank exit drive are not expected to be impacted if the proposed development access drive is placed to align with the entrance drive, rather than the exit drive.
- In both iterations of the study, the proposed development access drive is shown at an offset from the opposing bank drives. It is recommended generally that, if feasible, the new access drive should be aligned directly with the existing bank entrance drive. This is generally good practice to prevent conflicting turn movements. Because the bank drive currently permits entrance movements only, and because the proposed offset is in a direction that prevents overlap between NB LT and SB LT turn paths, this is less of an issue for this particular location. However, if the bank property is ever redeveloped in such a way that allows exiting traffic from this location, there may be turning movement conflicts between opposing exiting vehicles making lefthand turns onto SR 83. Direct alignment is also helpful for potential scenarios in which future development (onsite or offsite) creates a need for signalization at this location.

## Recommendations

The Harbor Crest TIS generally followed standard practices and appears to have been based on realistic assumptions. TranSystems recommends that the HCS calculations at the intersection of SR 83 and Walker Road should be rerun with pedestrian walk and clearance times incorporated to confirm that no adverse impacts are created in the Build condition. Additionally, consideration should be given to realigning the proposed development access drive on SR 83 to directly align with the existing bank entrance drive.



**SANITARY DISTRICT: S-4**

SANITARY SEWERS FOR THE ENTIRE PROJECT ARE ROUTED TO THE EXISTING 30" SANITARY SEWER IN WALKER ROAD.

**STORM DISTRICTS: HEIDER & TITUS-PITTS-HILL**

RUNOFF FROM THE MAJORITY OF THE PROJECT IS ROUTED TO A NEW RETENTION AND WATER QUALITY BASIN WHICH WILL DISCHARGE INTO THE TITUS-PITTS HILL DISTRICT TO AN EXISTING 48" STORM SEWER IN AVON BELDEN ROAD. A FRACTION OF THE PRE-DEVELOPMENT AREA THAT IS TRIBUTARY TO THE HEIDER DISTRICT IS ROUTED TO A WATER QUALITY BASIN WHICH WILL DISCHARGE INTO THE WALKER ROAD STORM SEWERS.

**STORM WATER MANAGEMENT:**

(EXCERPTED FROM MARCH 2025 PRELIMINARY STORM WATER MANAGEMENT PLAN)

IMPERVIOUS AREAS AND RUNOFF COEFFICIENTS:			
PRE-DEVEL HEIDER:	RESIDENTIAL DISTRICTS (2 ACRE)	C	0.51 77
	RESIDENTIAL DISTRICTS (2 ACRE)	D	1.81 82
	WOODS (GOOD)	C	0.67 70
	WOODS (GOOD)	D	1.10 77
	TOTAL AREA / WEIGHTED CN		4.09 78
PRE-DEVEL TITUS-P-H:	RESIDENTIAL DISTRICTS (2 ACRE)	C	0.44 77
	RESIDENTIAL DISTRICTS (2 ACRE)	D	6.10 82
	WOODS (GOOD)	C	0.58 70
	WOODS (GOOD)	D	0.65 77
	TOTAL AREA / WEIGHTED CN		7.77 80
POST DEVEL. HEIDER:	RESIDENTIAL DISTRICTS (1/4 ACRE)	D	2.27 87
	RESIDENTIAL DISTRICTS (1/8 ACRE)	C	2.21 90
POST DEVEL. TITUS-P-H:	RESIDENTIAL DISTRICTS (1/8 ACRE)	D	7.38 92
	RESIDENTIAL DISTRICTS (1/8 ACRE)	C	9.59 92

**CRITICAL STORM CALCULATIONS**

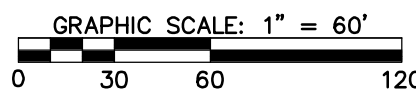
**HEIDER OUTLET POINT**  
PRE-DEVELOPMENT CONDITIONS  
AREA TRIBUTARY = 4.09 AC.  
WEIGHTED CN = 78 (SEE PAGE 2)  
TIME OF CONC. = 0.25 HR.  
1 YR. BEFORE DEVELOPMENT RUNOFF DEPTH = 0.471 IN.  
POST-DEVELOPMENT CONDITIONS  
AREA TRIBUTARY = 2.27 AC.  
2.27 AC. 1/4 ACRE LOTS, SOIL GROUP D, CN = 87  
TIME OF CONC. = 0.27 HR.  
1 YR. AFTER DEVELOPMENT RUNOFF DEPTH = 0.792 IN.  
INCREASE IN VOLUME =  $[(2.27)(.792) - (4.09)(0.471)] / (4.09)(0.471) = -7\%$   
NO RETENTION REQUIRED SINCE RUNOFF VOLUME DECREASES AFTER DEVEL.

**TITUS-PITTS-HILL OUTLET POINT**  
PRE-DEVELOPMENT CONDITIONS  
AREA TRIBUTARY = 7.77 AC.  
WEIGHTED CN = 80 (SEE PAGE 2)  
TIME OF CONC. = 0.33 HR.  
1 YR. BEFORE DEVELOPMENT RUNOFF DEPTH = 0.549 IN.  
POST-DEVELOPMENT CONDITIONS  
AREA TRIBUTARY = 9.59 AC.  
WEIGHTED CN = 92 (SEE PAGE 2)  
TIME OF CONC. = 0.38 HR.  
1 YR. AFTER DEVELOPMENT RUNOFF DEPTH = 1.215 IN.  
INCREASE IN VOLUME =  $[(9.59)(1.215) - (7.77)(0.549)] / (7.77)(0.549) = 173\%$   
CRITICAL STORM = 25 YEAR

**WATER QUALITY CALCULATIONS**

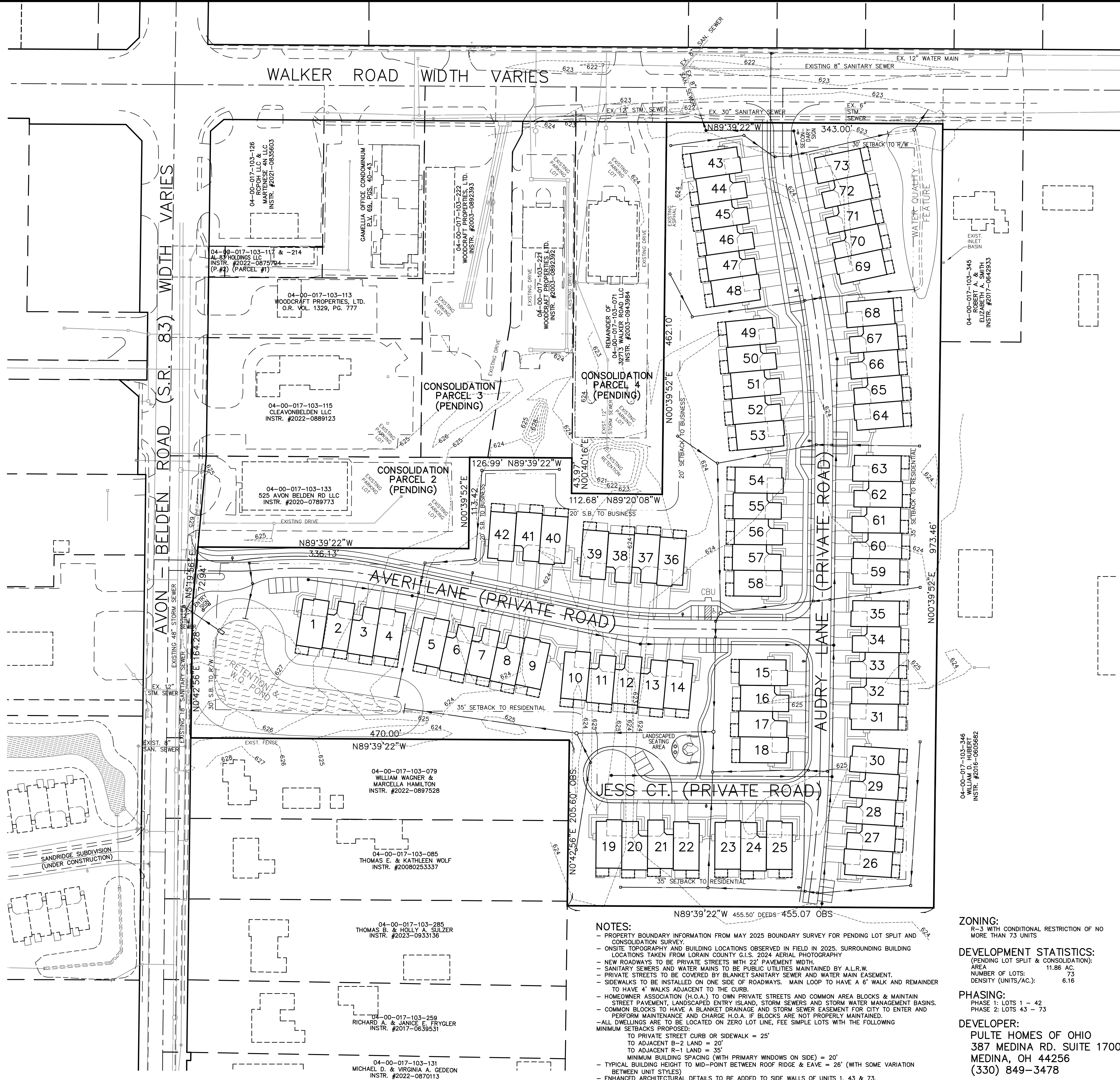
**HEIDER OUTLET POINT**  
IMPERVIOUS AREAS:  
BEFORE DEVELOPMENT: = 2% IMPERVIOUS  
AFTER DEVELOPMENT: = 50% IMPERVIOUS  
WATER QUALITY VOLUME REQUIRED:  
 $RV = 0.05 + 0.9(\text{FRACTION IMPERV.}, \text{AFTER DEV.})$   
 $WQV = (RV)(\text{TOTAL AREA TRIB.})(0.90"/12)$   
 $= (0.500)(9.59 \text{ AC.})(0.90"/12)$   
 $= 3.359 \text{ AC.-FT. } (X \ 43,560 = 15,640 \text{ CU. FT.})$

**TITUS-PITTS-HILL OUTLET POINT**  
IMPERVIOUS AREAS:  
BEFORE DEVELOPMENT: = 2% IMPERVIOUS  
AFTER DEVELOPMENT: = 50% IMPERVIOUS  
WATER QUALITY VOLUME REQUIRED:  
 $RV = 0.05 + 0.9(\text{FRACTION IMPERV.}, \text{AFTER DEV.})$   
 $WQV = (RV)(\text{TOTAL AREA TRIB.})(0.90"/12)$   
 $= (0.500)(2.27 \text{ AC.})(0.90"/12)$   
 $= 0.085 \text{ AC.-FT. } (X \ 43,560 = 3,700 \text{ CU. FT.})$



**LEGEND:**

- SETBACK LINE (SEE NOTES)
- M.H. EXISTING SANITARY SEWER
- M.H. EXISTING STORM SEWER
- M.H. EXISTING WATER MAIN
- M.H. PROPOSED SANITARY SEWER
- M.H. PROPOSED STORM SEWER
- M.H. PROPOSED WATER MAIN
- TYPICAL BUILDING LAYOUT



- NOTES:**
- PROPERTY BOUNDARY INFORMATION FROM MAY 2025 BOUNDARY SURVEY FOR PENDING LOT SPLIT AND CONSOLIDATION SURVEY
  - ON-SITE TOPOGRAPHY AND BUILDING LOCATIONS OBSERVED IN FIELD IN 2025. SURROUNDING BUILDING LOCATIONS TAKEN FROM LORAIN COUNTY G.I.S. 2024 AERIAL PHOTOGRAPHY
  - NEW ROADWAYS TO BE PRIVATE STREETS WITH 22' PAVEMENT WIDTH
  - SANITARY SEWERS AND WATER MAINS TO BE PUBLIC UTILITIES MAINTAINED BY A.L.R.W.
  - PRIVATE STREETS TO BE COVERED BY BLANKET SANITARY SEWER AND WATER MAIN EASEMENT.
  - SIDEWALKS TO BE INSTALLED ON ONE SIDE OF ROADWAYS. MAIN LOOP TO HAVE A 6' WALK AND REMAINDER TO HAVE 4' WALKS ADJACENT TO THE CURB.
  - HOMEOWNER ASSOCIATION (H.O.A.) TO OWN PRIVATE STREETS AND COMMON AREA BLOCKS & MAINTAIN STREET PAVEMENT, LANDSCAPED ENTRY ISLAND, STORM SEWERS AND STORM WATER MANAGEMENT BASINS.
  - COMMON BLOCKS TO HAVE A BLANKET DRAINAGE AND STORM SEWER EASEMENT FOR CITY TO ENTER AND PERFORM MAINTENANCE AND CHARGE H.O.A. IF BLOCKS ARE NOT PROPERLY MAINTAINED.
  - ALL DWELLINGS ARE TO BE LOCATED ON ZERO LOT LINE, FEE SIMPLE LOTS WITH THE FOLLOWING MINIMUM SETBACKS PROPOSED:
    - TO PRIVATE STREET CURB OR SIDEWALK = 25'
    - TO ADJACENT B-2 LAND = 20'
    - TO ADJACENT R-1 LAND = 35'
    - MINIMUM BUILDING SPACING (WITH PRIMARY WINDOWS ON SIDE) = 20'
    - TYPICAL BUILDING HEIGHT TO MID-POINT BETWEEN ROOF RIDGE & EAVE = 26' (WITH SOME VARIATION BETWEEN UNIT STYLES)
    - ENHANCED ARCHITECTURAL DETAILS TO BE ADDED TO SIDE WALLS OF UNITS 1, 43 & 73.

**ZONING:**  
R-3 WITH CONDITIONAL RESTRICTION OF NO MORE THAN 73 UNITS

**DEVELOPMENT STATISTICS:**  
(PENDING LOT SPLIT & CONSOLIDATION):  
AREA 11.86 AC.  
NUMBER OF LOTS: 73  
DENSITY (UNITS/AC.): 6.16

**PHASING:**  
PHASE 1: LOTS 1 - 42  
PHASE 2: LOTS 43 - 73

**DEVELOPER:**  
PULTE HOMES OF OHIO  
387 MEDINA RD. SUITE 1700  
MEDINA, OH 44256  
(330) 849-3478

THE HENRY G. REITZ  
ENGINEERING COMPANY  
4214 ROCKY RIVER DRIVE, CLEVELAND, OH, 44135  
PHONE: (216) 251-3033, REITZ@REITZENG.COM

HARBOR CREST  
TOWNHOUSE SUBDIVISION  
PRELIMINARY PLAT

1/1  
MAY 2025

REVISIONS	REVISED PER ZONING DEPARTMENT COMMENTS, PHASING & LOT NUMBERS
6/20/2025	



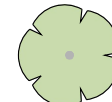
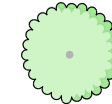


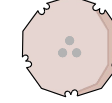

GENERAL NOTES:

1. EACH CONTRACTOR IS TO VERIFY WITH OWNER AND UTILITY COMPANIES THE LOCATIONS OF ALL UTILITIES PRIOR TO CONSTRUCTION, TO DETERMINE IN THE FIELD THE ACTUAL LOCATIONS AND ELEVATIONS OF ALL EXISTING UTILITIES, WHETHER SHOWN ON THE PLANS OR NOT. THE CONTRACTOR SHALL CALL UTILITY PROTECTION SERVICE 72 HOURS PRIOR TO CONSTRUCTION.
2. EXAMINE FINISH SURFACE, GRADES, TOPSOIL QUALITY AND DEPTH. DO NOT START ANY WORK UNTIL UNSATISFACTORY CONDITIONS HAVE BEEN CORRECTED. VERIFY LIMITS OF WORK BEFORE STARTING.
3. CONTRACTOR RESPONSIBLE FOR COST OF REPAIRS TO EXISTING SITE CONDITIONS WHEN DAMAGED BY CONTRACTOR. REPAIR TO THE SATISFACTION OF THE OWNER.
4. ALL PLANT MASSES TO BE CONTAINED WITHIN 3" DEEP HARDWOOD MULCH BED WITH SMOOTH/CONTINUOUS SHOVEL/DUG EDGE WHEN NOT BOUND BY PAVEMENT.
5. CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE IN LAWN AREAS.
6. FINE GRADE LAWN AREAS TO PROVIDE A SMOOTH AND CONTINUAL GRADE FREE OF IRREGULARITIES OR DEPRESSIONS.
7. CONTRACTOR SHALL SEED OR SOD ALL AREAS DISTURBED DURING CONSTRUCTION.
8. ALL PLANTS SHALL MEET OR EXCEED STANDARDS SET IN THE U.S.A. STANDARD FOR NURSERY STOCK.
9. ALL PLANTING OPERATIONS SHALL ADHERE TO THE AMERICAN ASSOCIATION OF NURSERYMEN STANDARDS.
10. QUANTITIES SHOWN ARE INTENDED TO ASSIST CONTRACTOR IN EVALUATING THEIR OWN TAKE OFFS AND ARE NOT GUARANTEED AS ACCURATE REPRESENTATIONS OF REQUIRED MATERIALS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THEIR BID QUANTITIES AS REQUIRED BY THE PLAN.
11. WHERE TREE LOCATIONS OCCUR UNDER EXISTING OVERHEAD UTILITIES OR CROWD EXISTING TREES, NOTIFY OWNER'S REPRESENTATIVE TO ADJUST TREE LOCATIONS.

PLANTING CONSTRUCTION NOTES:

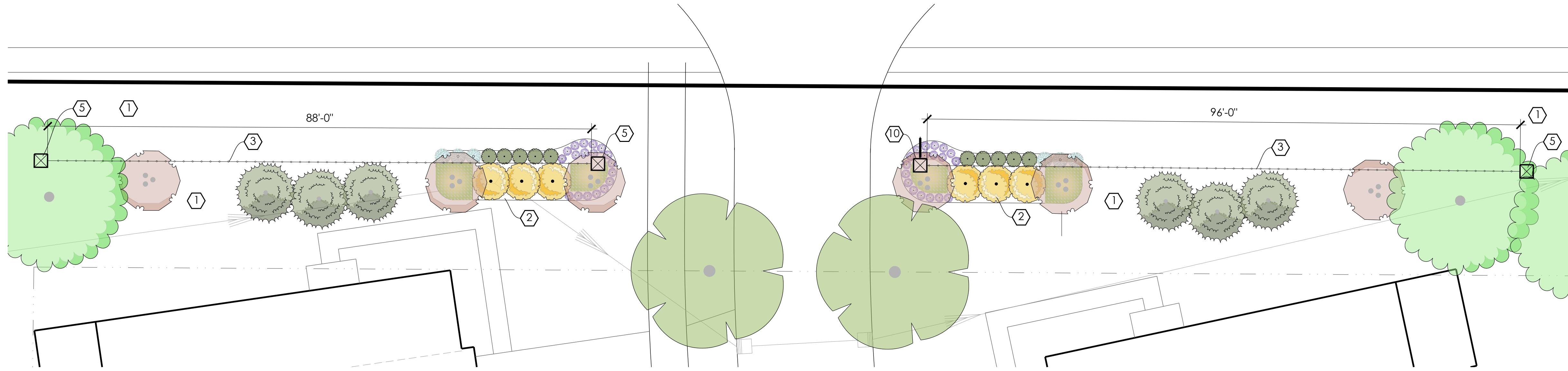
- 1 LAWN AREA, TOPSOIL TO BE FLUSH WITH ADJACENT PAVED SURFACES. SEED OR SOD TO BE PLACED AFTER PLANTING.
- 2 LANDSCAPE AREA, PLANT AS INDICATED AND MULCH ENTIRE AREA WITH HARDWOOD BARK MULCH AT 3" MIN.
- 6 6' HEIGHT VINYL PRIVACY FENCE. SEE DETAIL 3 ON SHEET L-5.

PLANT SCHEDULE OVERALL LANDSCAPE

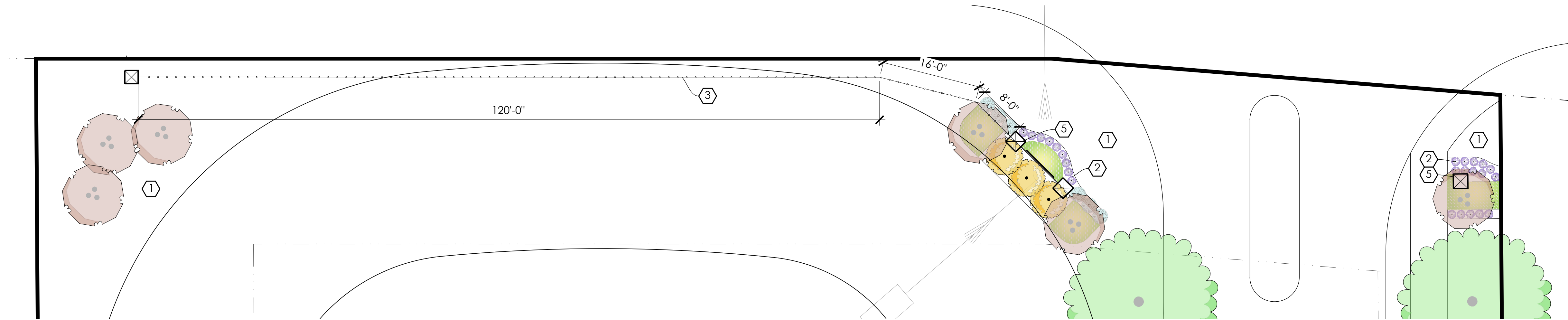
	<b>DECIDUOUS SHADE TREE TYP 1</b>	<b>66</b>
	<b>DECIDUOUS SHADE TREE TYP 2</b>	<b>24</b>
	<b>LG EVERGREEN TREE</b>	<b>27</b>
	<b>SM EVERGREEN TREE</b>	<b>35</b>
	<b>SM ORNAMENTAL TREE</b>	<b>27</b>
	<b>LG DECIDUOUS SHRUB</b>	<b>47</b>



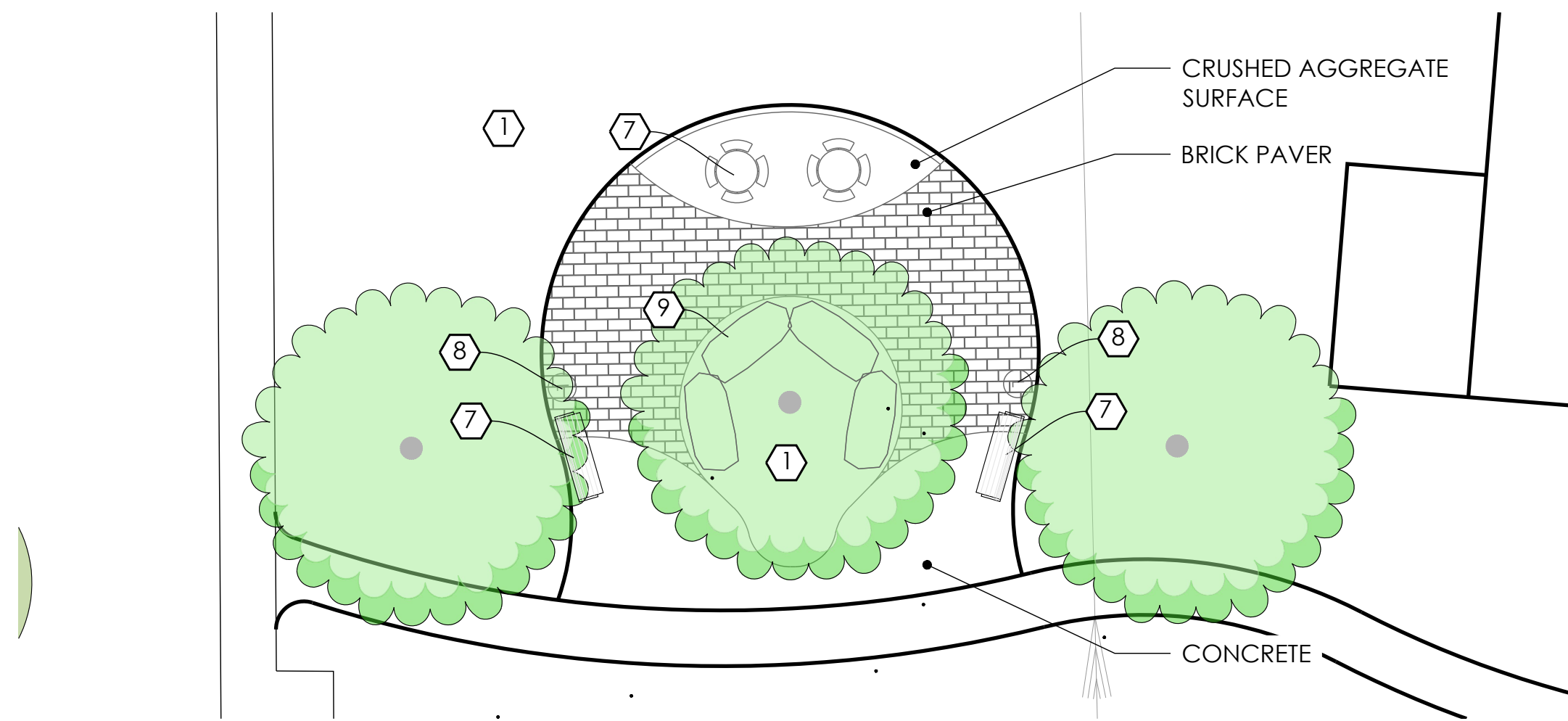
PRELIMINARY  
NOT FOR CONSTRUCTION



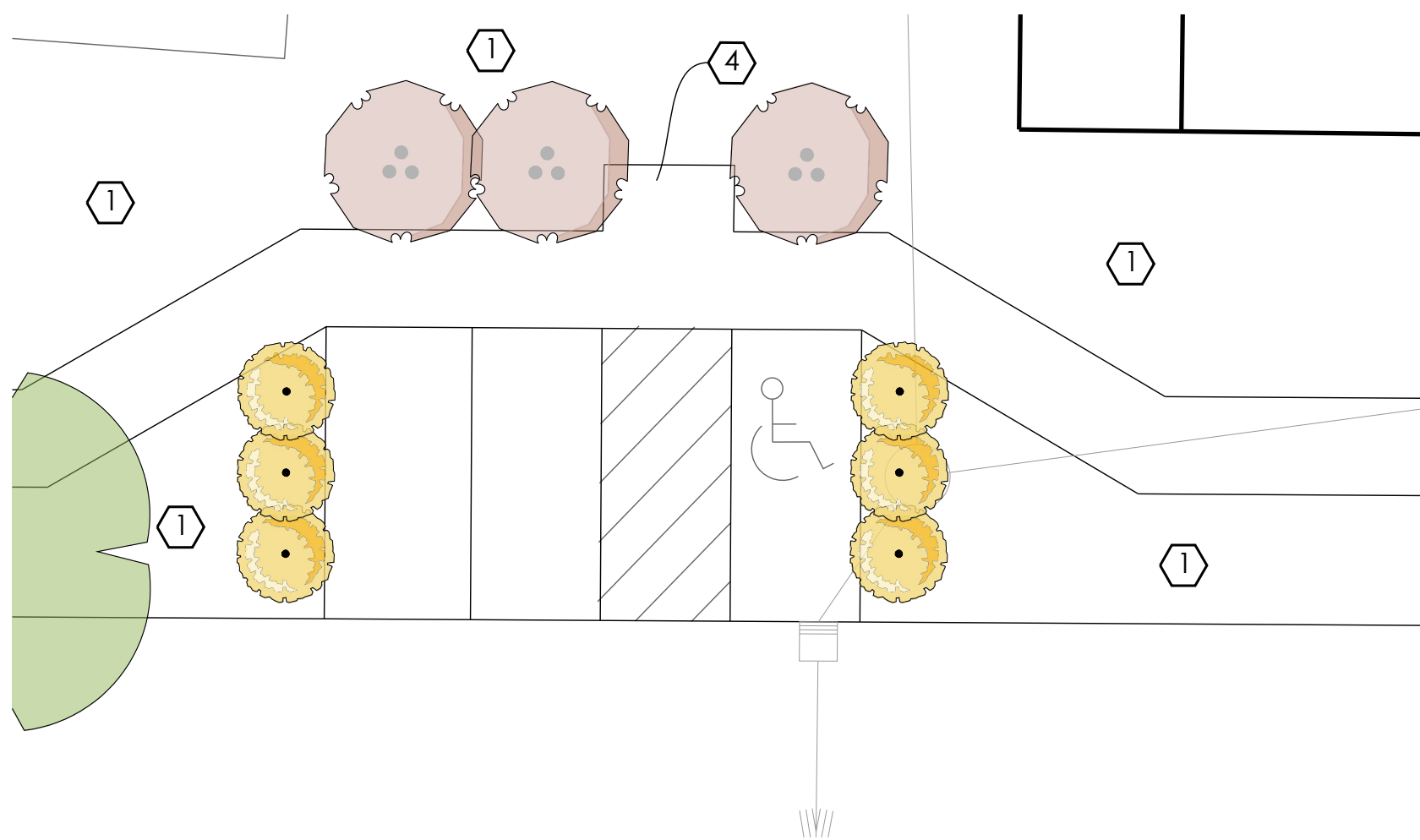
1 WALKER ROAD ENTRY



2 AVON BELDEN ROAD ENTRY



3 POCKET PARK



4 CLUSTER BOX UNIT



GENERAL NOTES:

1. EACH CONTRACTOR IS TO VERIFY WITH OWNER AND UTILITY COMPANIES THE LOCATIONS OF ALL UTILITIES PRIOR TO CONSTRUCTION, TO DETERMINE IN THE FIELD THE ACTUAL LOCATIONS AND ELEVATIONS OF ALL EXISTING UTILITIES, WHETHER SHOWN ON THE PLANS OR NOT. THE CONTRACTOR SHALL CALL UTILITY PROTECTION SERVICE 72 HOURS PRIOR TO CONSTRUCTION.
2. EXAMINE FINISH SURFACE, GRADES, TOPSOIL QUALITY AND DEPTH. DO NOT START ANY WORK UNTIL UNSATISFACTORY CONDITIONS HAVE BEEN CORRECTED. VERIFY LIMITS OF WORK BEFORE STARTING.
3. CONTRACTOR RESPONSIBLE FOR COST OF REPAIRS TO EXISTING SITE CONDITIONS WHEN DAMAGED BY CONTRACTOR. REPAIR TO THE SATISFACTION OF THE OWNER.
4. ALL PLANT MASSES TO BE CONTAINED WITHIN 3" DEEP HARDWOOD MULCH BED WITH SMOOTH/CONTINUOUS SHOVEL/DUG EDGE WHEN NOT BOUND BY PAVEMENT.
5. CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE IN LAWN AREAS.
6. FINE GRADE LAWN AREAS TO PROVIDE A SMOOTH AND CONTINUAL GRADE FREE OF IRREGULARITIES OR DEPRESSIONS.
7. CONTRACTOR SHALL SEED OR SOD ALL AREAS DISTURBED DURING CONSTRUCTION.
8. ALL PLANTS SHALL MEET OR EXCEED STANDARDS SET IN THE U.S.A. STANDARD FOR NURSERY STOCK.
9. ALL PLANTING OPERATIONS SHALL ADHERE TO THE AMERICAN ASSOCIATION OF NURSERYMEN STANDARDS.
10. QUANTITIES SHOWN ARE INTENDED TO ASSIST CONTRACTOR IN EVALUATING THEIR OWN TAKE OFFS AND ARE NOT GUARANTEED AS ACCURATE REPRESENTATIONS OF REQUIRED MATERIALS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THEIR BID QUANTITIES AS REQUIRED BY THE PLAN.
11. WHERE TREE LOCATIONS OCCUR UNDER EXISTING OVERHEAD UTILITIES OR CROWD EXISTING TREES, NOTIFY OWNER'S REPRESENTATIVE TO ADJUST TREE LOCATIONS.

PLANTING CONSTRUCTION NOTES:

- 1 LAWN AREA, TOPSOIL TO BE FLUSH WITH ADJACENT PAVED SURFACES. SEED OR SOD TO BE PLACED AFTER PLANTING.
- 2 LANDSCAPE AREA, PLANT AS INDICATED AND MULCH ENTIRE AREA WITH HARDWOOD BARK MULCH AT 3" MIN.
- 3 4' HEIGHT ALUMINUM FENCE. SEE DETAIL 1 ON SHEET L-5.
- 4 CLUSTER BOX UNIT. SEE DETAIL 2 ON SHEET L-5.
- 5 ENTRY SIGN AND COLUMNS. SEE DETAILS 8 & 9 ON SHEET L-4.
- 6 6' HEIGHT VINYL PRIVACY FENCE. SEE DETAIL 3 ON SHEET L-5.
- 7 SEATING. SEE DETAILS 4 & 5 ON SHEET L-5.
- 8 WASTE RECEPTACLE. SEE DETAIL 6 ON SHEET L-5.
- 9 BOULDER. SEE DETAIL 6 ON SHEET L-4.
- 10 SECONDARY ENTRY SIGN. SEE DETAIL 10 ON SHEET L-4.

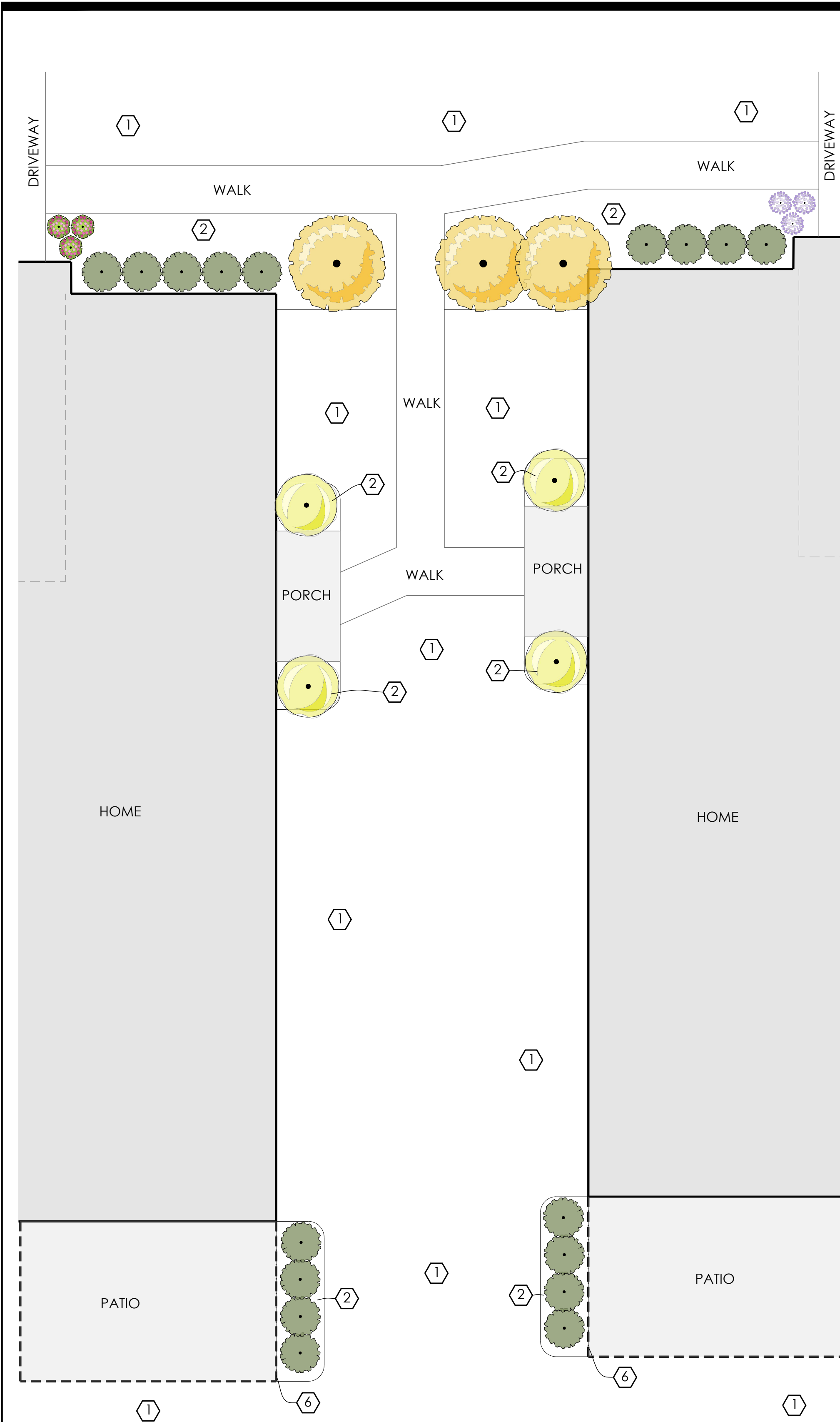
PLANTING - WALKER ROAD ENTRY

	<u>SM ORNAMENTAL TREE</u>	4
	<u>LG DECIDUOUS SHRUB</u>	6
	<u>SM EVERGREEN SHRUB</u>	10
	<u>PERENNIAL TYP 1</u>	38
	<u>ORNAMENTAL GRASS TYP 1</u>	6
	<u>GROUND COVER TYP 1</u>	112 SF

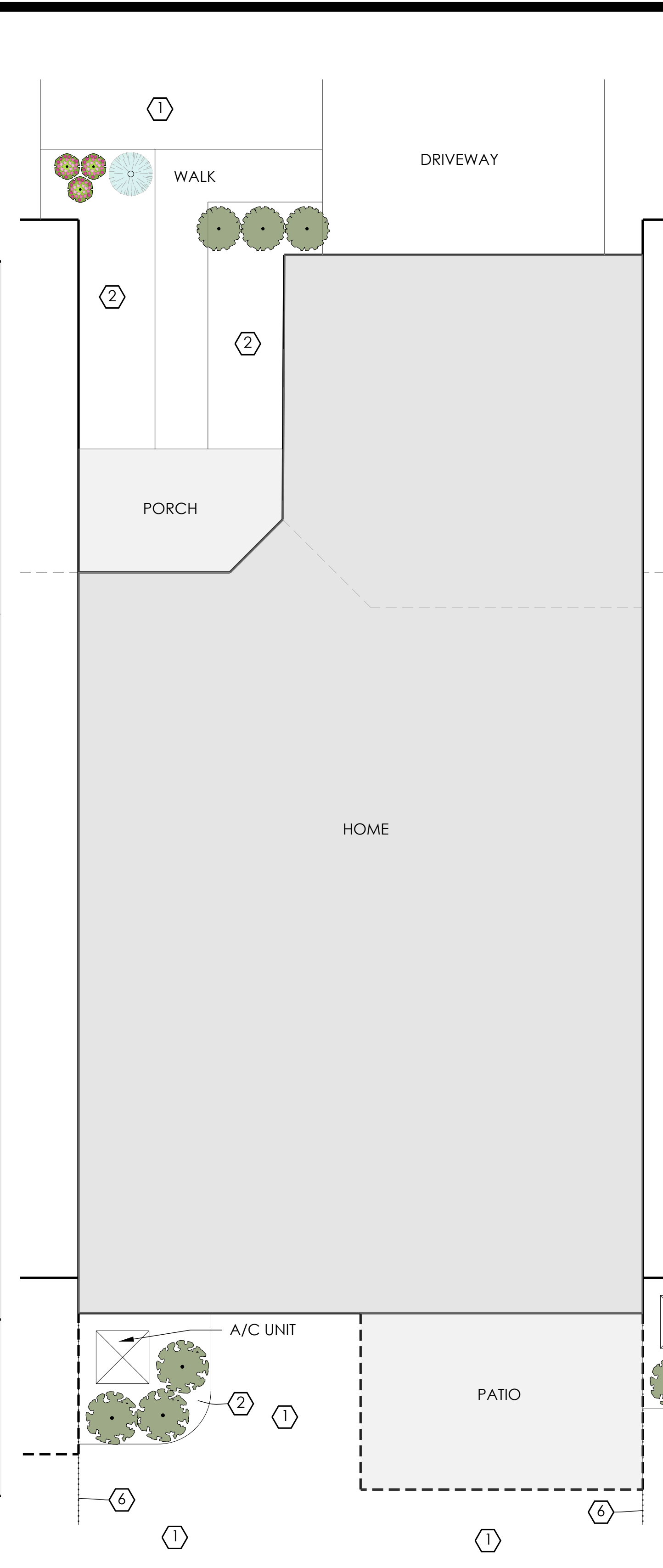
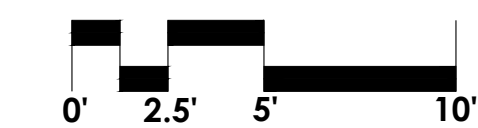
PLANTING - AVON BELDEN ROAD ENTRY

	<u>SM ORNAMENTAL TREE</u>	3
	<u>LG DECIDUOUS SHRUB</u>	3
	<u>PERENNIAL TYP 1</u>	25
	<u>ORNAMENTAL GRASS TYP 1</u>	6
	<u>GROUND COVER TYP 1</u>	126 SF

PRELIMINARY  
NOT FOR CONSTRUCTION

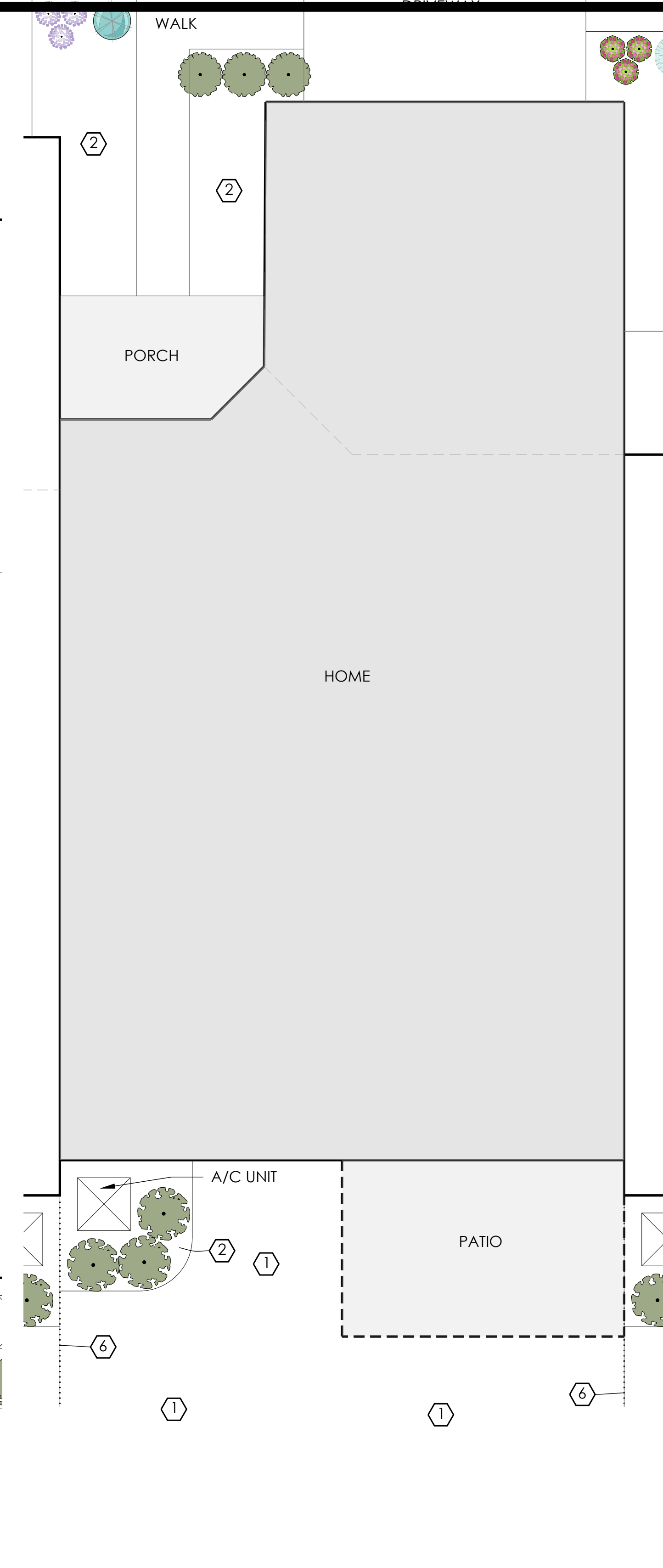
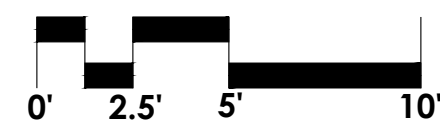


1 TYPICAL END UNIT PLANTING



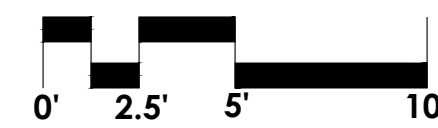
2 TYPICAL INTERIOR UNIT PLANTING A

TYPICAL OF 40 UNITS



3 TYPICAL INTERIOR UNIT PLANTING B

TYPICAL OF 33 UNITS



GENERAL NOTES:

1. EACH CONTRACTOR IS TO VERIFY WITH OWNER AND UTILITY COMPANIES THE LOCATIONS OF ALL UTILITIES PRIOR TO CONSTRUCTION, TO DETERMINE IN THE FIELD THE ACTUAL LOCATIONS AND ELEVATIONS OF ALL EXISTING UTILITIES, WHETHER SHOWN ON THE PLANS OR NOT. THE CONTRACTOR SHALL CALL UTILITY PROTECTION SERVICE 72 HOURS PRIOR TO CONSTRUCTION.
2. EXAMINE FINISH SURFACE, GRADES, TOPSOIL QUALITY AND DEPTH. DO NOT START ANY WORK UNTIL UNSATISFACTORY CONDITIONS HAVE BEEN CORRECTED. VERIFY LIMITS OF WORK BEFORE STARTING.
3. CONTRACTOR RESPONSIBLE FOR COST OF REPAIRS TO EXISTING SITE CONDITIONS WHEN DAMAGED BY CONTRACTOR. REPAIR TO THE SATISFACTION OF THE OWNER.
4. ALL PLANT MASSES TO BE CONTAINED WITHIN 3" DEEP HARDWOOD MULCH BED WITH SMOOTH/CONTINUOUS SHOVEL/DUG EDGE WHEN NOT BOUND BY PAVEMENT.
5. CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE IN LAWN AREAS.
6. FINE GRADE LAWN AREAS TO PROVIDE A SMOOTH AND CONTINUAL GRADE FREE OF IRREGULARITIES OR DEPRESSIONS.
7. CONTRACTOR SHALL SEED OR SOD ALL AREAS DISTURBED DURING CONSTRUCTION.
8. ALL PLANTS SHALL MEET OR EXCEED STANDARDS SET IN THE U.S.A. STANDARD FOR NURSERY STOCK.
9. ALL PLANTING OPERATIONS SHALL ADHERE TO THE AMERICAN ASSOCIATION OF NURSERYMEN STANDARDS.
10. QUANTITIES SHOWN ARE INTENDED TO ASSIST CONTRACTOR IN EVALUATING THEIR OWN TAKE OFFS AND ARE NOT GUARANTEED AS ACCURATE REPRESENTATIONS OF REQUIRED MATERIALS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THEIR BID QUANTITIES AS REQUIRED BY THE PLAN.
11. WHERE TREE LOCATIONS OCCUR UNDER EXISTING OVERHEAD UTILITIES OR CROWD EXISTING TREES, NOTIFY OWNER'S REPRESENTATIVE TO ADJUST TREE LOCATIONS.

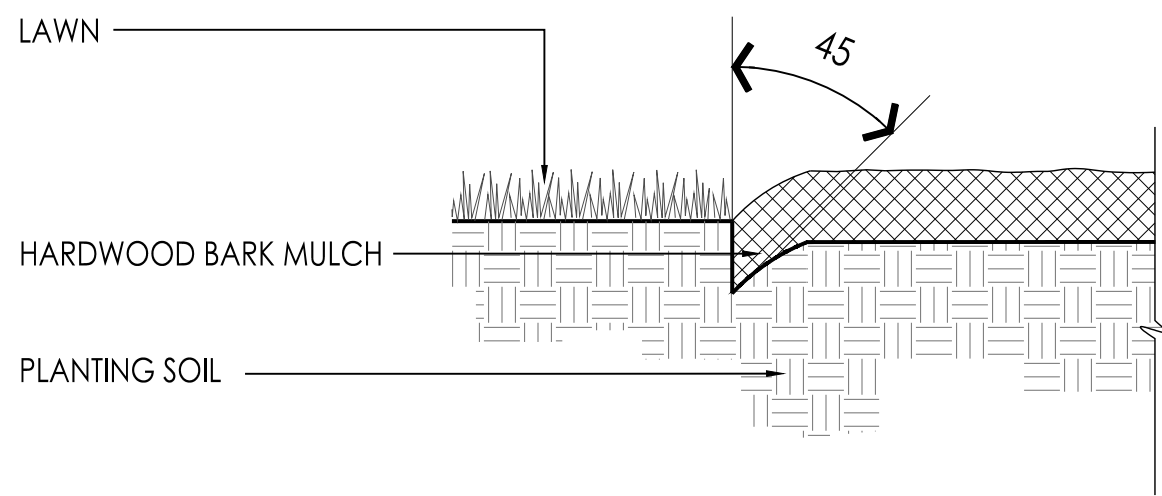
PLANTING CONSTRUCTION NOTES:

- 1 LAWN AREA, TOPSOIL TO BE FLUSH WITH ADJACENT PAVED SURFACES. SEED OR SOD TO BE PLACED AFTER PLANTING.
- 2 LANDSCAPE AREA, PLANT AS INDICATED AND MULCH ENTIRE AREA WITH HARDWOOD BARK MULCH AT 3" MIN.
- 6 6' HEIGHT VINYL PRIVACY FENCE. SEE DETAIL 3 ON SHEET L-5.

PLANTING - ALL UNITS

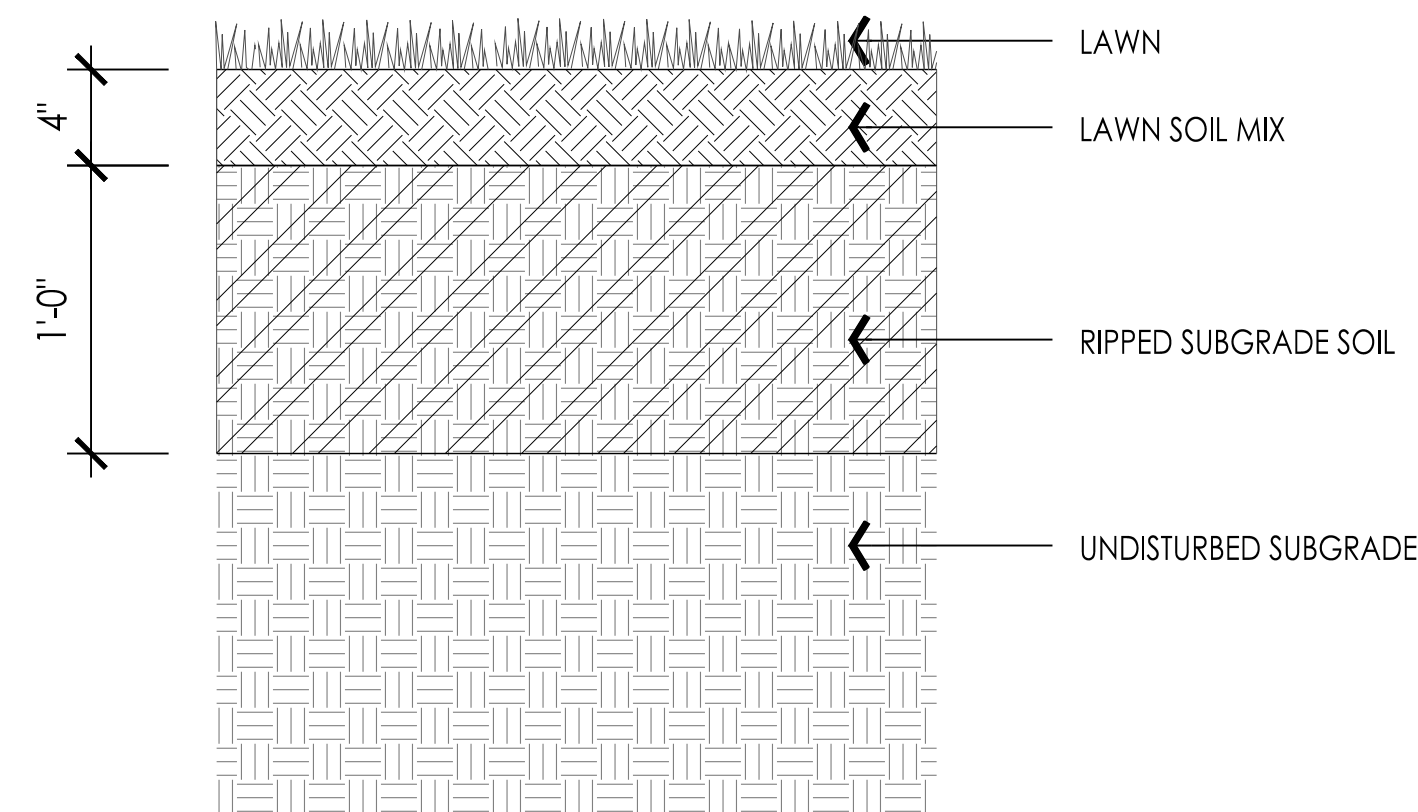
	<b>LG DECIDUOUS SHRUB</b>	<b>34</b>
	<b>SM DECIDUOUS SHRUB</b>	<b>64</b>
	<b>LG EVERGREEN SHRUB</b>	<b>219</b>
	<b>SM EVERGREEN SHRUB</b>	<b>409</b>
	<b>PERENNIAL TYP 1</b>	<b>99</b>
	<b>PERENNIAL TYP 2</b>	<b>120</b>
	<b>ORNAMENTAL GRASS TYP 1</b>	<b>26</b>
	<b>ORNAMENTAL GRASS TYP 2</b>	<b>15</b>

PRELIMINARY  
NOT FOR CONSTRUCTION



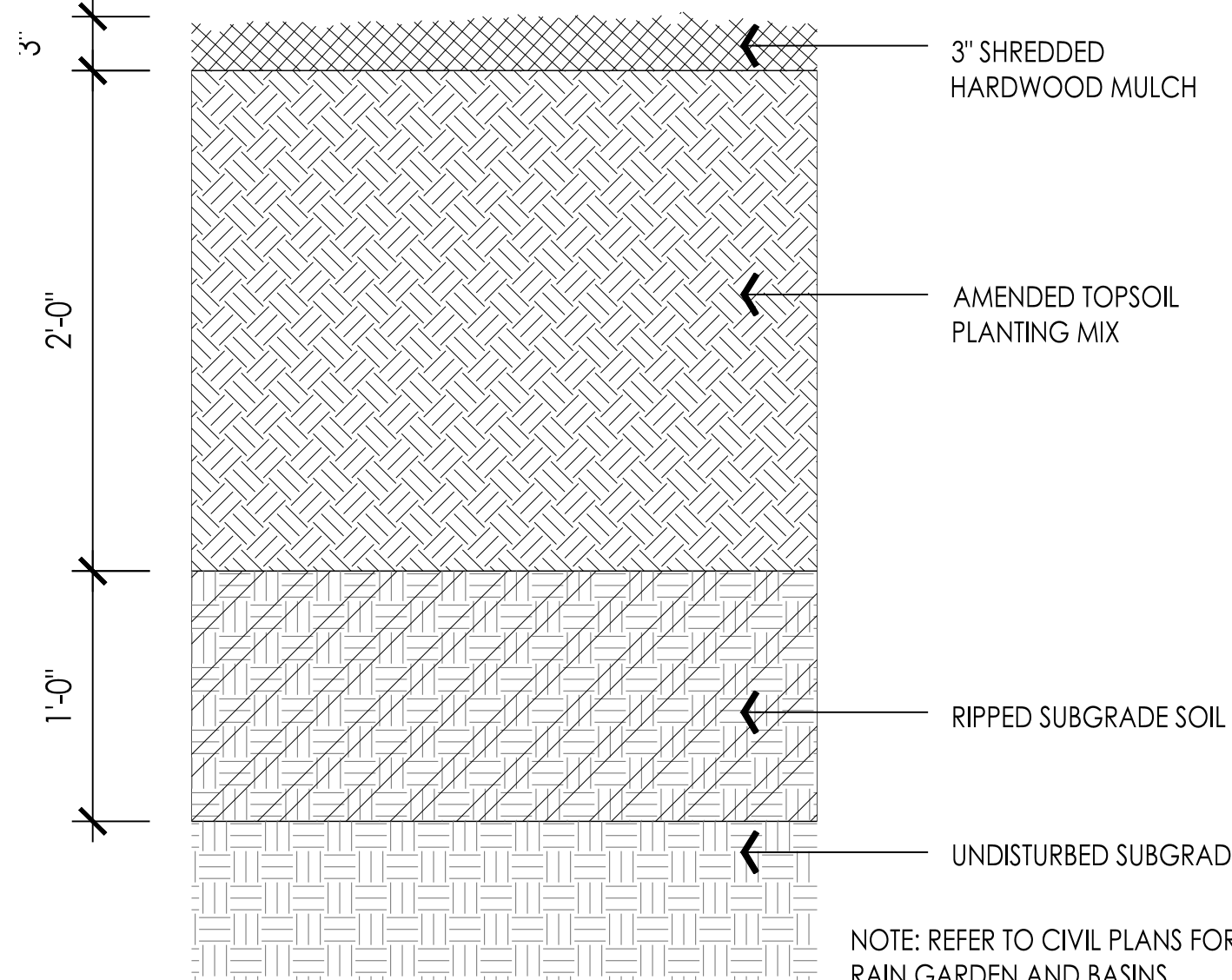
## 1 PLANT BED EDGE

N.T.S.



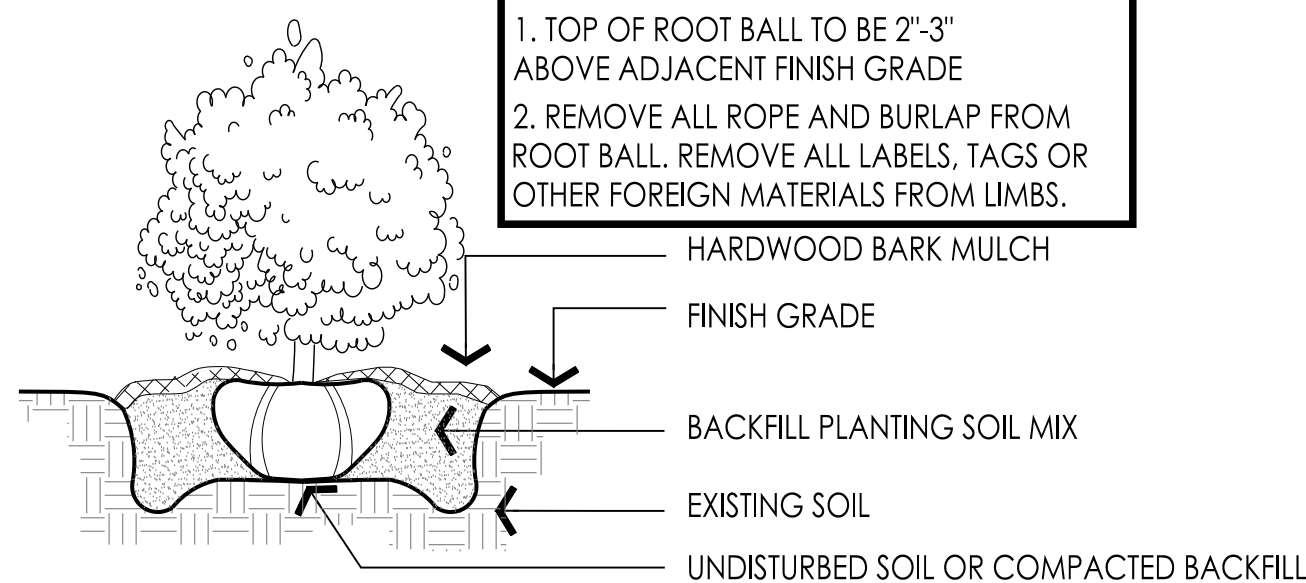
## 2 SOIL PROFILE AT LAWN

N.T.S.



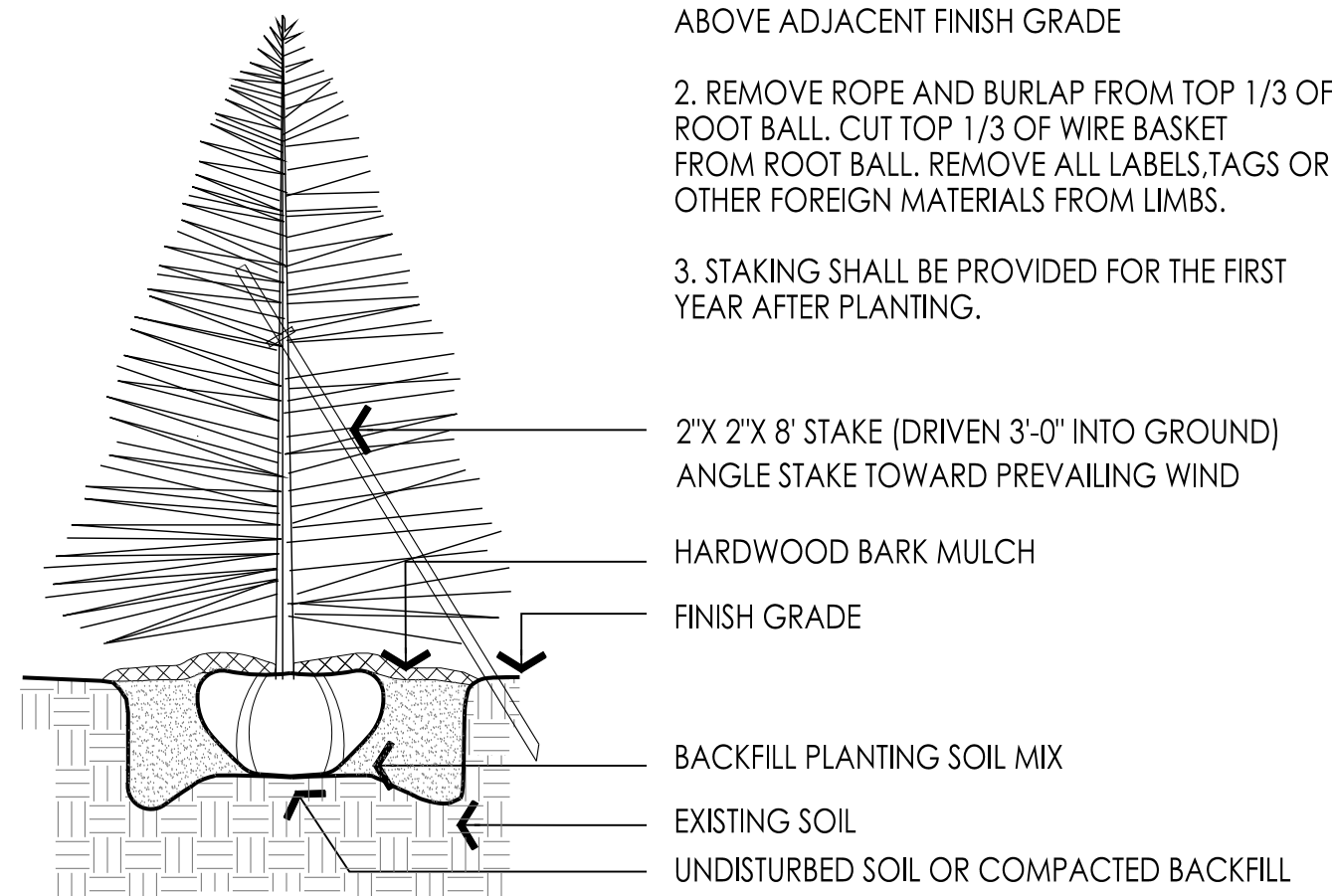
## 3 SOIL PROFILE AT PLANT BED

N.T.S.



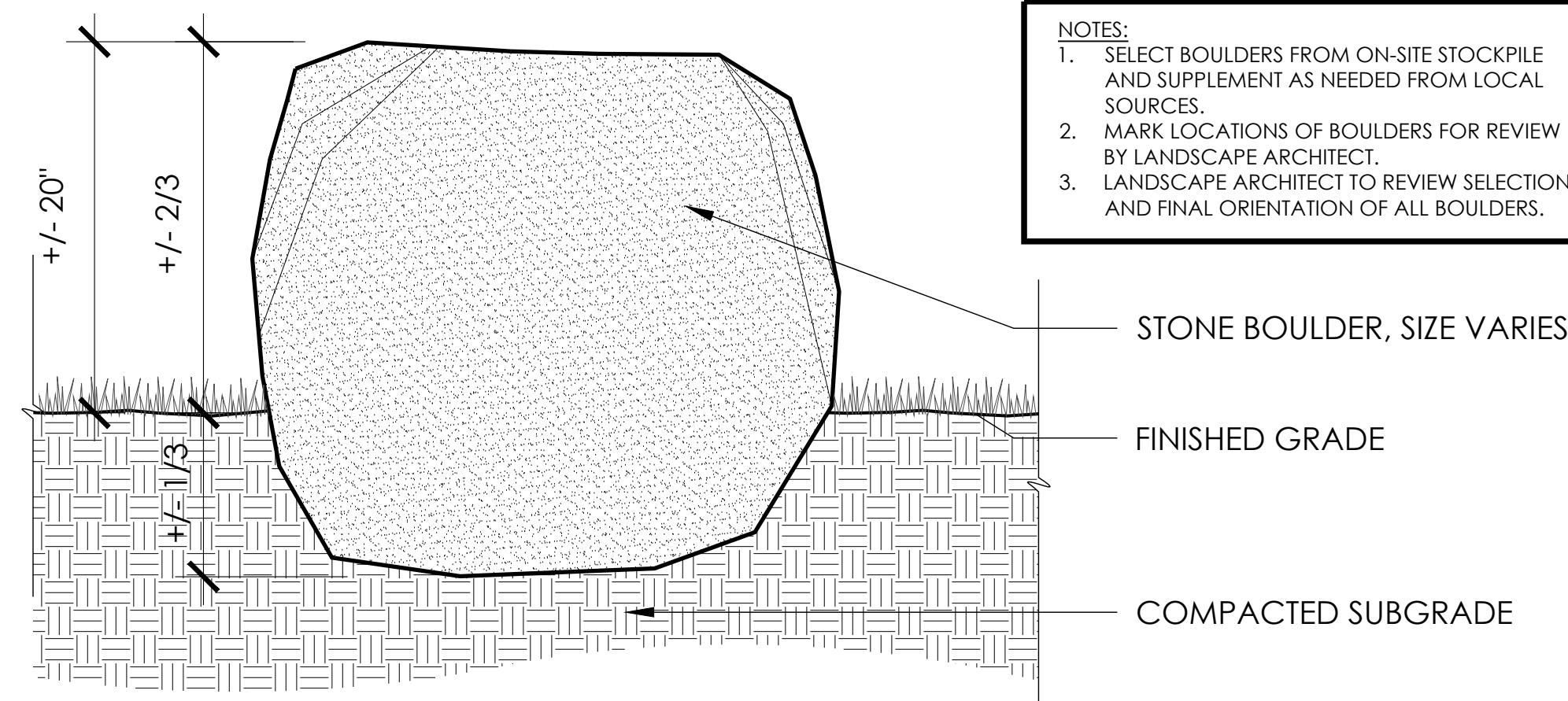
## 4 SHRUB PLANTING

N.T.S.



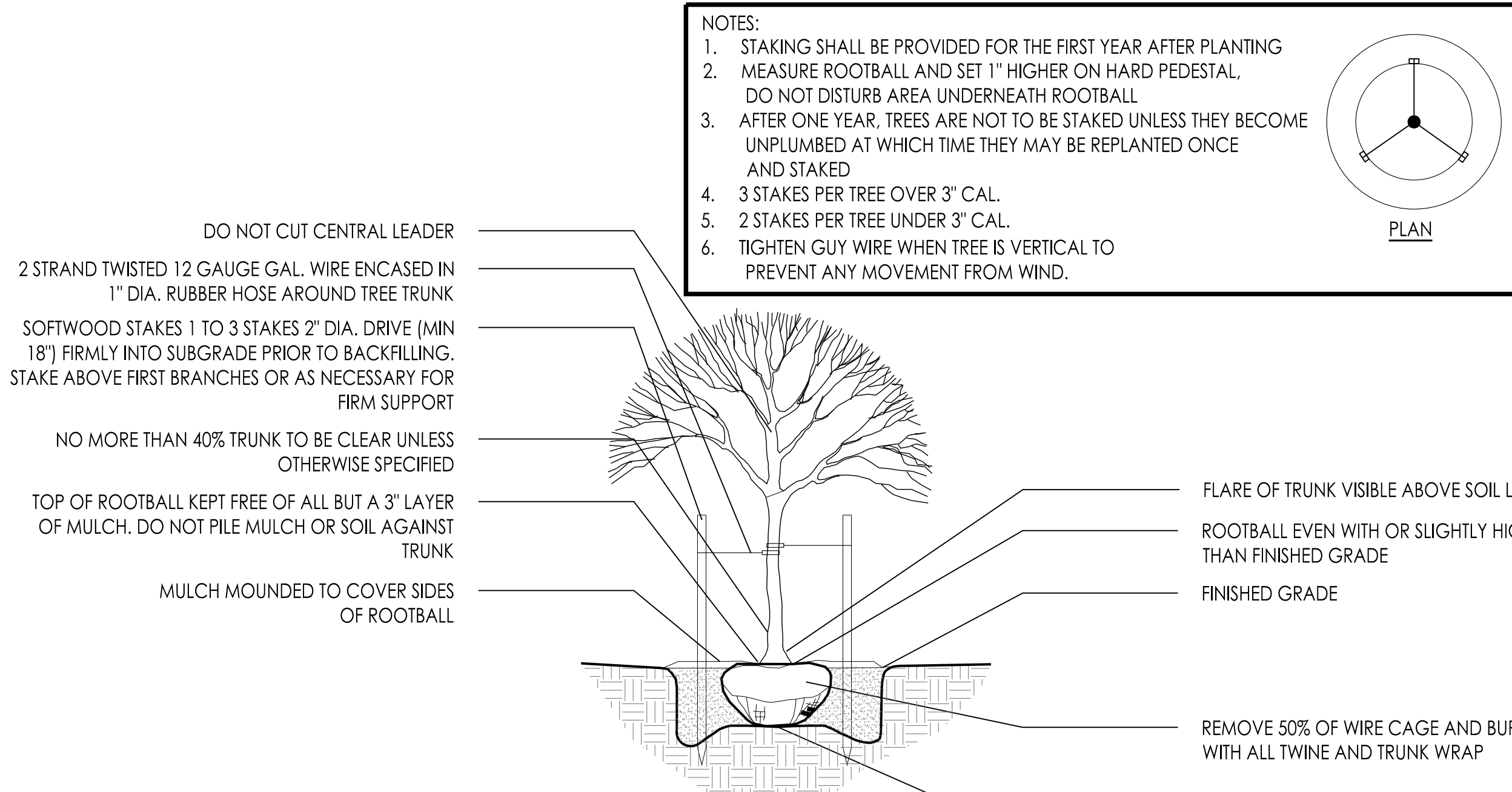
## 5 EVERGREEN TREE PLANTING

N.T.S.



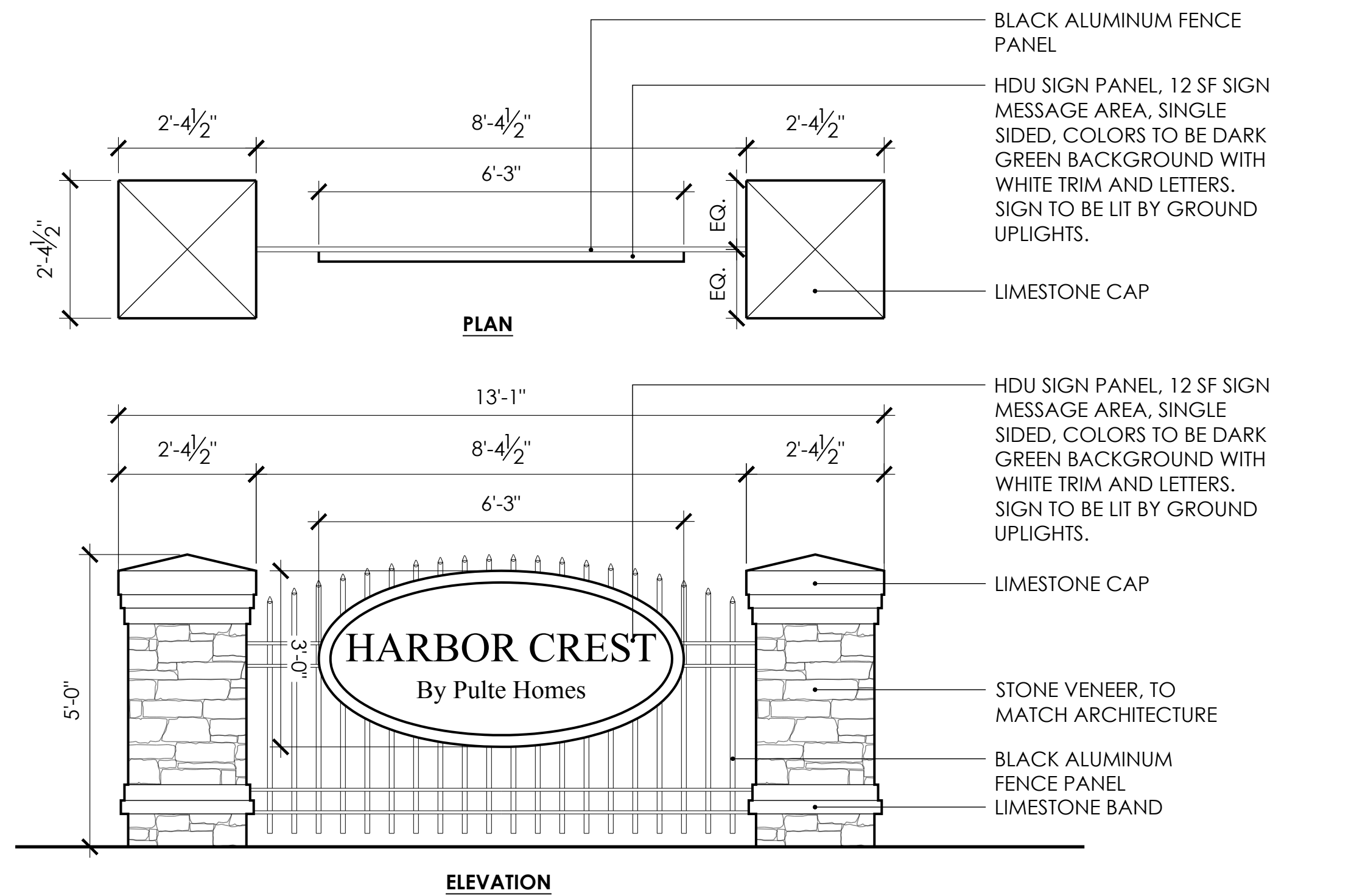
## 6 STONE BOULDER

N.T.S.



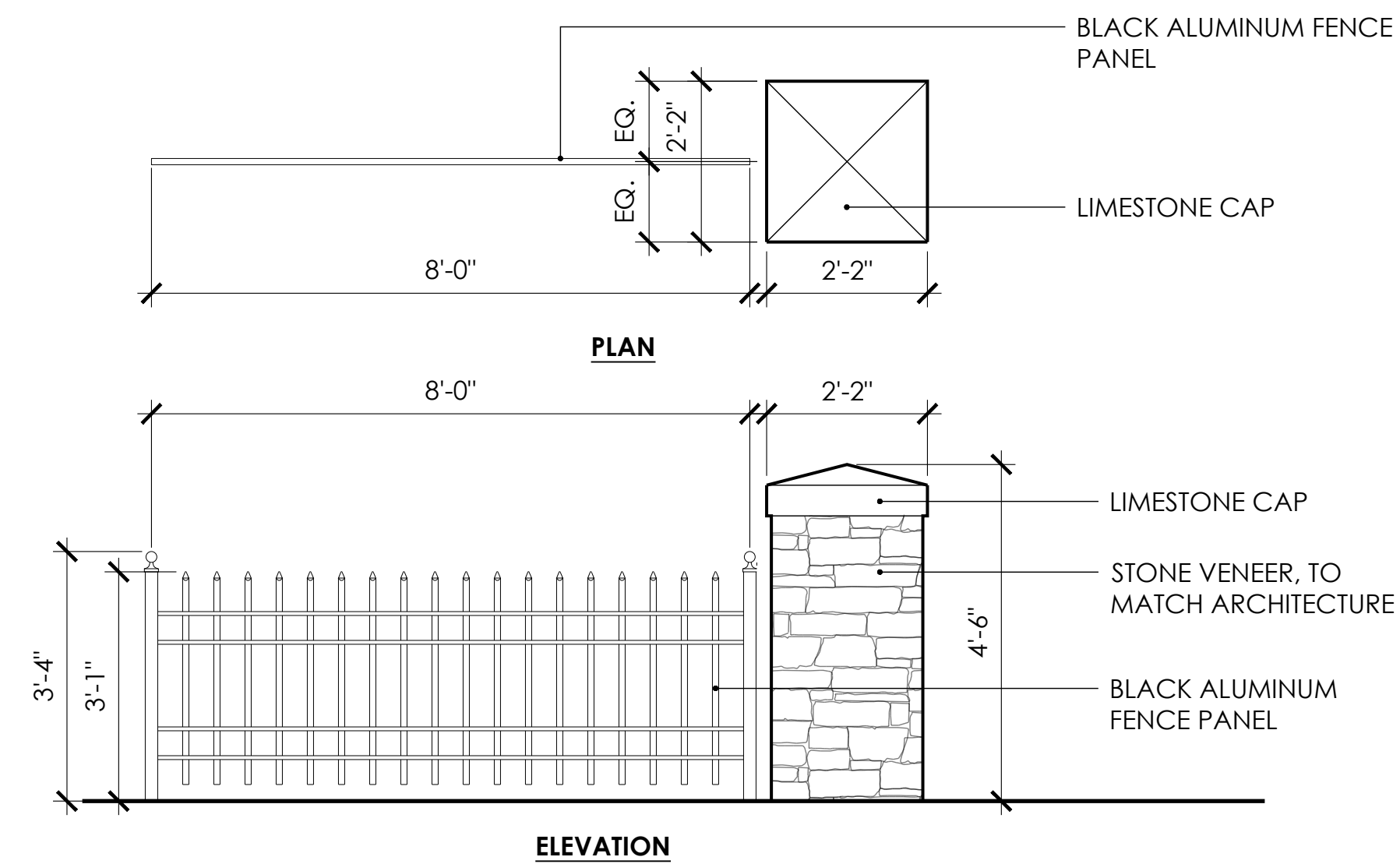
## 7 DECIDUOUS TREE PLANTING

N.T.S.



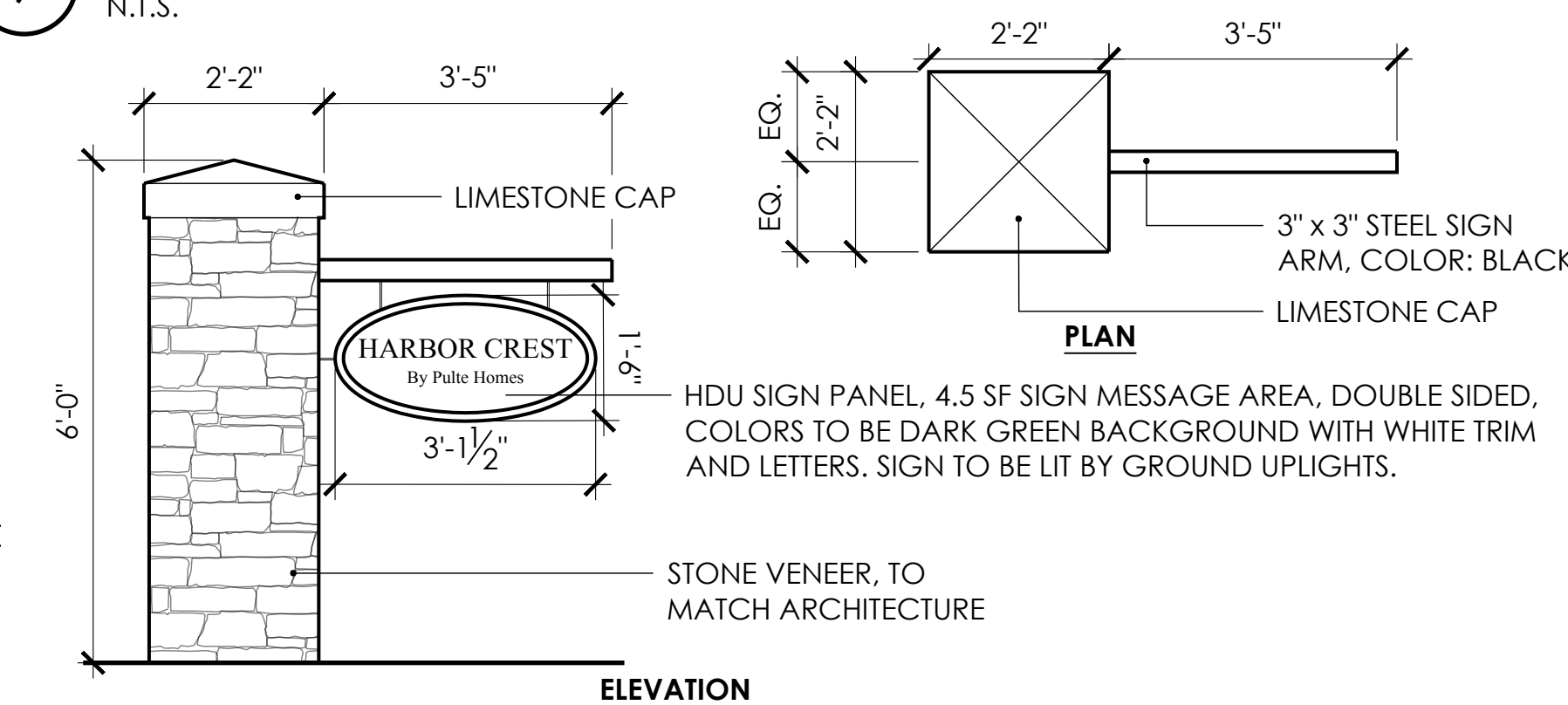
## 8 ENTRY SIGN DETAIL

N.T.S.



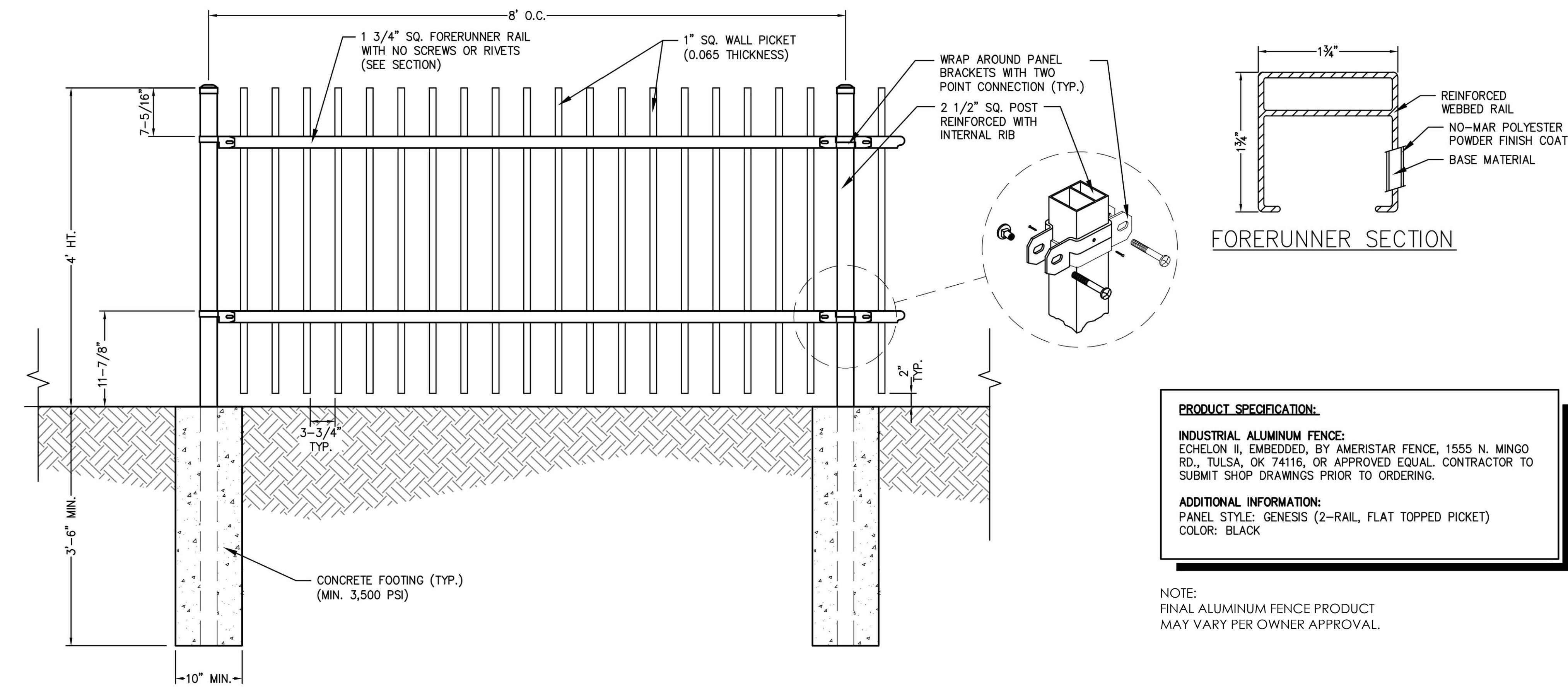
## 9 FENCE AND TERMINATING COLUMN

N.T.S.

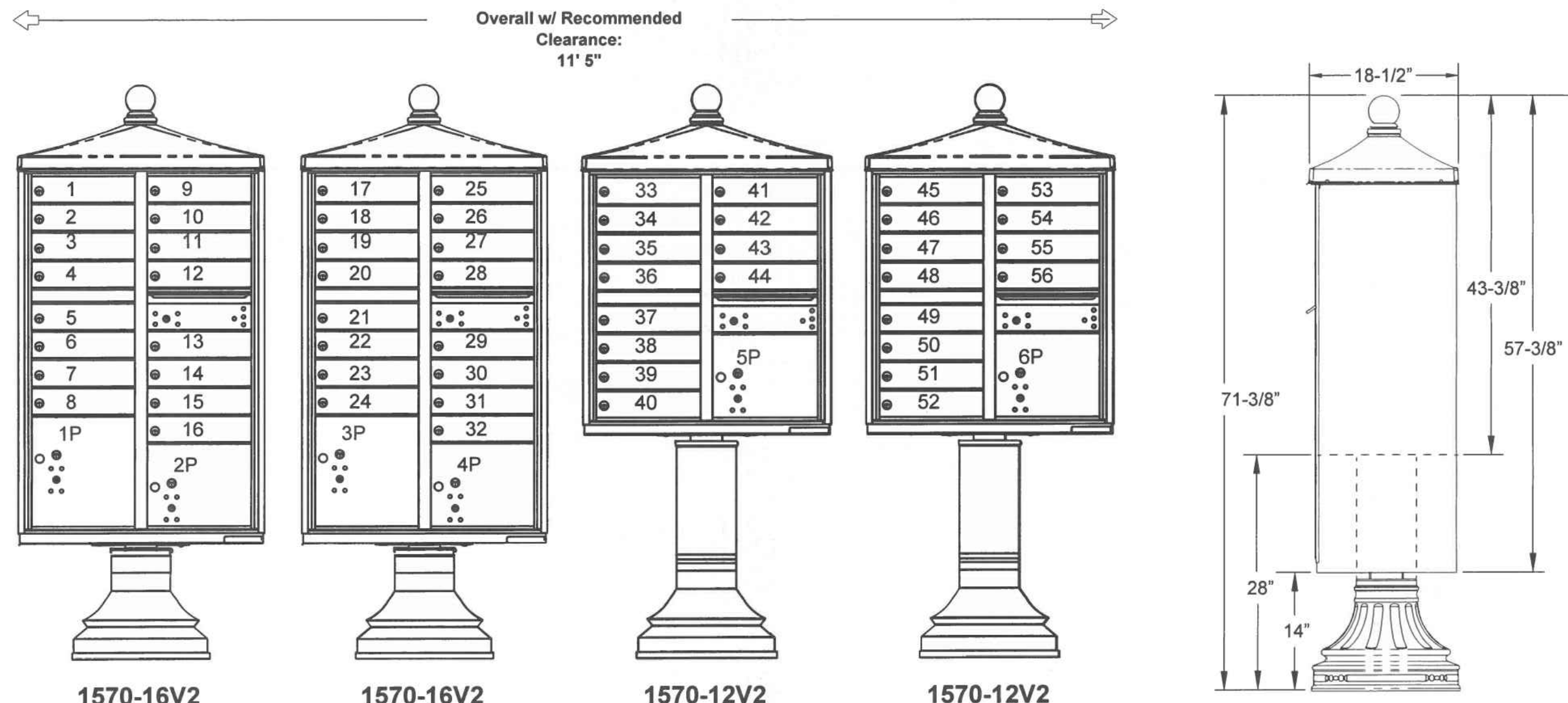


## 10 SECONDARY ENTRY SIGN

N.T.S.

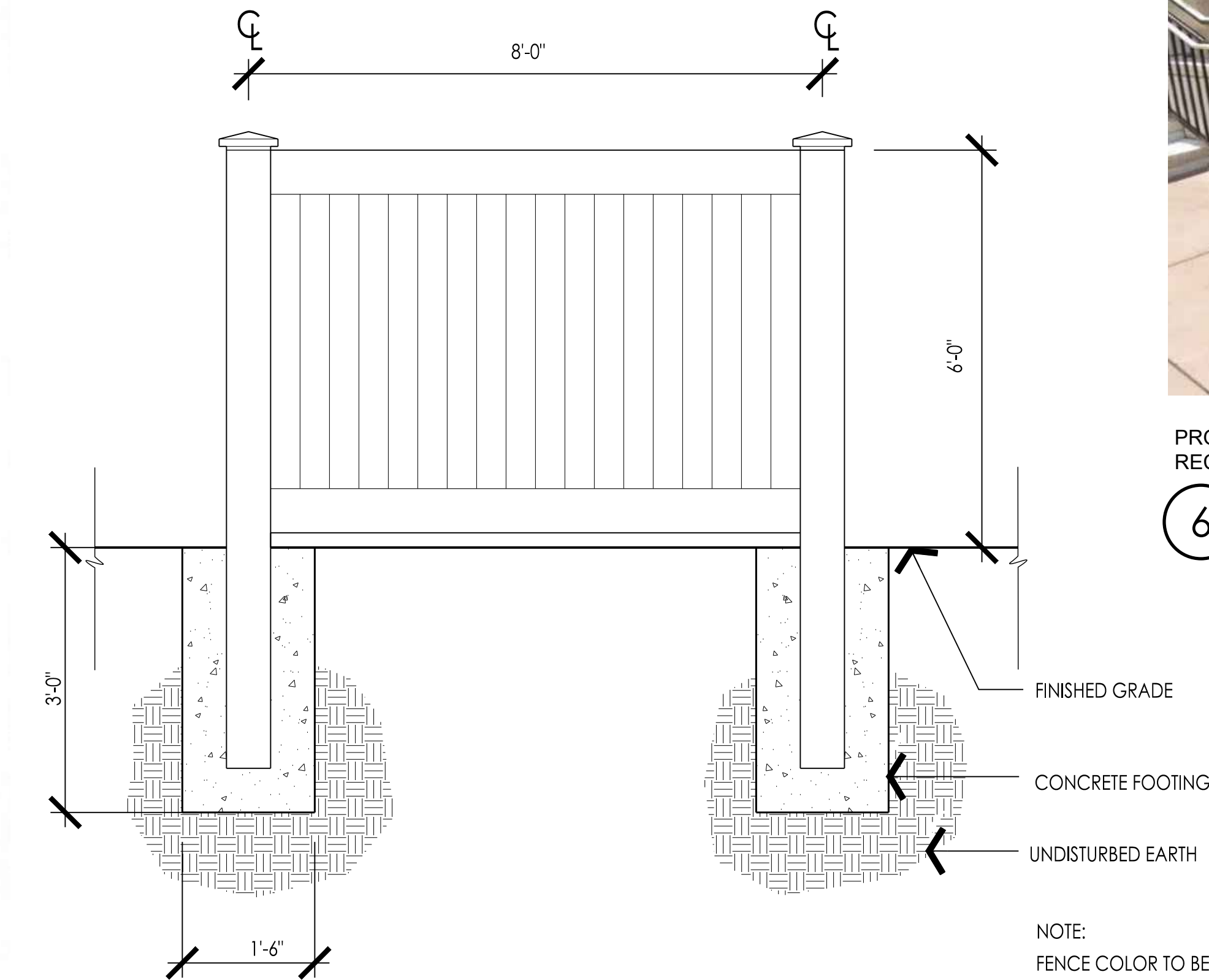


1 4' HEIGHT ALUMINUM FENCE  
N.T.S.



Configuration Details:	Models Used:	Door Sizes Used:
1. Product Type: Cluster Box Units 2. Installation: Mounted on a pedestal 3. Finish: Black 4. Locks: Standard Cam Lock, 3 keys 5. Door Id: Decals 6. Master Door: Prepared for USPS Master Lock Note: Number placement on drawing does not represent the final position of the engraving/decals.	(2) 1570-16V2 (2) 1570-12V2	(86) 3" H Tenant (2) 13-3/8" H Parcel (4) 9-7/8" H Parcel
DO NOT SCALE OFF DRAWING		
NOTE: FINAL CBU PRODUCT AND COLOR MAY VARY PER OWNER APPROVAL.		

2 CLUSTER BOX UNIT DETAIL  
N.T.S.



3 6' HEIGHT VINYL PRIVACY FENCE  
N.T.S.



PRODUCT: TREE TOP PRODUCTS - STELLA ALUMINUM BENCH, 6' LENGTH, BLACK OR OWNER APPROVED EQUAL.

4 BENCH SEATING  
N.T.S.



PRODUCT: TREE TOP PRODUCTS - SUPERSAVER COMMERCIAL ROUND PICNIC TABLE OR OWNER APPROVED EQUAL.

5 PICNIC TABLES  
N.T.S.



PRODUCT: TREE TOP PRODUCTS - COMMERCIAL STEEL WASTE RECEPTACLE OR OWNER APPROVED EQUAL.

6 WASTE RECEPTACLE  
N.T.S.

PRELIMINARY  
NOT FOR CONSTRUCTION



## FORD MOTOR COMPANY OHAP- SITE PLAN

# Report

**To:** Avon Lake Planning Commission

**From:** Kelly La Rosa, Planning and Zoning Manager

**Date:** July 30, 2025

**Re:** Case No. CPC-25-11, Ford Motor Company, Ohio Assembly Plant, Site Plan Approval for the Construction of a New Vehicle Test Track and Installation of a New Stormwater Retention Basin.

---

### PROJECT OVERVIEW

Ford Motor Company seeks site plan approval for two integrated infrastructure improvements at its Ohio Assembly Plant (OHAP): (1) the construction of a new vehicle test track and (2) the installation of a new stormwater retention basin. Both components are located within the Avon Lake portion of the Ford campus, which straddles the Avon Lake/Sheffield Lake municipal boundary.

The proposed improvements are intended to enhance operational efficiency and reduce stormwater runoff burden on the existing internal system, which has historically been prone to flooding. These projects are located on a single parcel and are being submitted as one unified site plan application.

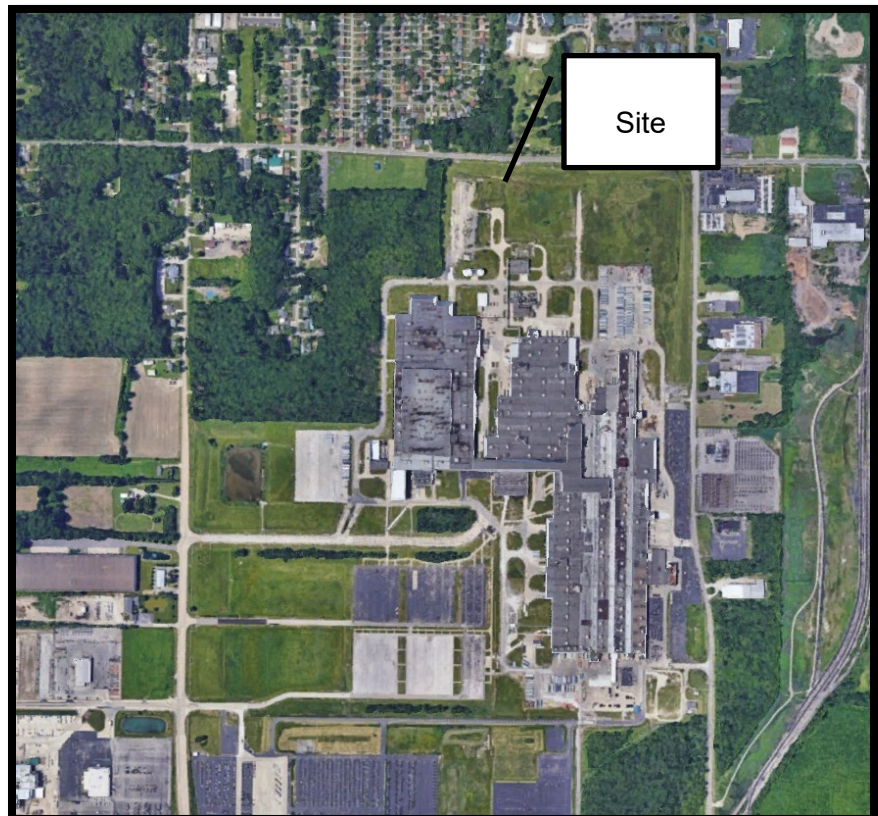


Figure 1: Maps Data: Google Earth 6/2/2015.



Planning Commission  
Case No. CPC-25-11  
Ford Motor Company OHAP  
Site Plan Approval  
July 30, 2025  
Page 2 of 6

## **PROJECT DESCRIPTION**

### **1. Test Track ("Clearvision Track")**

The project includes the construction of an approximately 808-foot-long, 30-foot-wide concrete vehicle test track with a cul-de-sac-style turnaround. The track will be used for in-line production testing, particularly for verifying wheel alignment. The project will also include:

- Demolition of existing concrete and light poles
- Grading and site work
- Installation of stormwater structures, catch basins, and piping
- New lighting mounted on 20-foot poles with LED fixtures, directed to minimize light spill
- Installation of signage and approximately 300 linear feet of jersey barriers to segregate the test area from adjacent operations
- Pavement striping per Ford Motor Company standards

Since the proposed test track crosses the municipal boundary between Sheffield Lake and Avon Lake, Ford will request separate approvals from each jurisdiction. Avon Lake will only review the section within its limits.

### **2. Stormwater Retention Pond ("East Pond Project")**

A new retention pond will be installed in the southeast corner of the plant. This improvement is part of a multi-phase strategy to address longstanding internal flooding issues by alleviating pressure on the A-line storm sewer system. This phase includes:

- Rerouting of roof drains to a new underground stormwater sewer system
- Construction of an approximately 40,000 square foot retention pond
- Installation of 1,400 linear feet of stormwater pipe, catch basins, manholes, a headwall, and an overflow structure
- Installation of new fencing and guardrails
- Future phases will incorporate a Biomatrix floating wetland to enhance water quality and provide ecological benefits.

This phase builds on prior work that rerouted parking lot stormwater directly to the Miller Road outlet. The retention pond will be connected to the same rerouted system.



Planning Commission  
Case No. CPC-25-11  
Ford Motor Company OHAP  
Site Plan Approval  
July 30, 2025  
Page 3 of 6

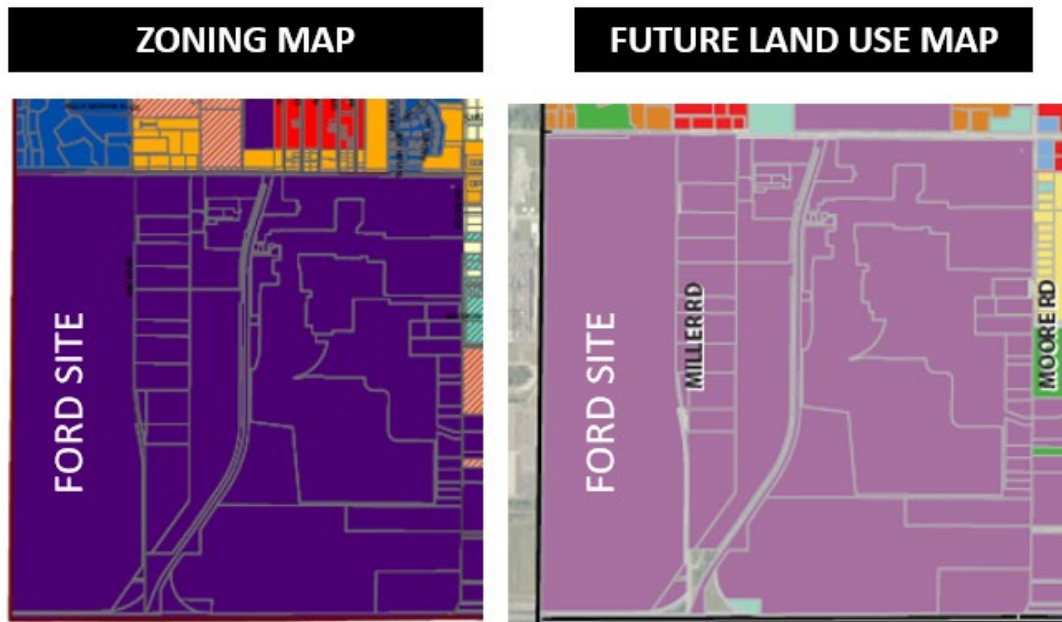


Figure 2: Excerpt from Avon Lake Zoning and Future Land Use Maps

**Zoning Map:** This property is in the I-2 General Industrial Zoning District. To the north, across the street, is an R-3 Multi-Family District. Another I-2 General Industrial area lies to the east, with Sheffield Village to the west and Avon to the south.

The I-2 District is intended for industrial uses that must be separated from residential areas due to potential impacts like noise, fumes, and traffic. These areas support essential city functions, including employment, and are located where the land, utilities, and transportation systems are best suited for industrial development.

#### **Comprehensive Land Use Plan:**

The Future Land Use Map identifies this site as Industrial and is intended for light industrial users that require space for wholesale, warehousing, clean manufacturing, packaging, repair, and related office functions.

**Applicable Code Section:** 1214.06 Site Plans apply.



Planning Commission  
Case No. CPC-25-11  
Ford Motor Company OHAP  
Site Plan Approval  
July 30, 2025  
Page 4 of 6

## **PROJECT ANALYSIS**

### **Zoning and Land Use**

The site is located within the I-2 General Industrial District. The proposed improvements are consistent with permitted uses in this district and are in line with the intent of the zoning code to support ongoing industrial operations.

The Avon Lake Comprehensive Land Use Plan identifies this area as part of a long-term industrial corridor.

### **Sustainability Considerations**

The proposed project incorporates several sustainability features that align with both operational efficiency and environmental stewardship goals.

For the test track, sustainability efforts include the installation of LED lighting, which will reduce energy consumption while maintaining adequate visibility and safety for vehicle testing. In addition, the project will improve stormwater management by redirecting existing swales to new storm structures, reducing surface runoff, and improving drainage in the immediate area.

The retention pond serves as a key stormwater mitigation strategy, capturing and holding runoff from rooftop drains. This approach will alleviate pressure on both the facility's internal storm system and the broader city stormwater infrastructure, particularly during heavy rainfall events. In future phases, the pond may be enhanced by the installation of a Biomatix floating wetland, a vegetative system designed to improve water quality by naturally filtering stormwater.

Site preparation is expected to have minimal environmental impact, limited to grading and essential site work, with no significant vegetation removal anticipated. This limited environmental impact further supports the project's overall sustainability profile.

### **Development Review Committee**

The Development Review Committee (DRC) comprises representatives from the Community Development, Building, Public Works, Avon Lake Regional Water, Fire, and Police Departments. During the review process, no objections were raised by any of the participating departments regarding the proposed improvements. However, the Engineering Department's review is still in progress, and a formal comment letter from the City Engineer is pending. This letter is expected to address the completeness and adequacy of the site's stormwater management design, among other technical considerations.



Planning Commission  
Case No. CPC-25-11  
Ford Motor Company OHAP  
Site Plan Approval  
July 30, 2025  
Page 5 of 6

## **REVIEW AND RECOMMENDATION BY THE COMMISSION**

The Commission shall review the application, and in reviewing the application, the Commission shall, at a minimum, consider the review criteria in Section 1214.06 (d).

### **Review Criteria**

To approve a site plan, the Planning Commission shall determine that:

- (1) The proposed development is consistent with all the requirements of this code and other related codes and ordinances of the City;
- (2) The proposed development complies with the applicable zoning district regulations;
- (3) The proposed development complies with any established standards or requirements in the approved comprehensive land use plan or thoroughfare plan;
- (4) The proposed development meets all the requirements or conditions of any applicable development approvals (e.g., previously approved planned developments, conditional use approvals, variance approvals, etc.);
- (5) The development will result in a harmonious grouping of buildings within the proposed development and in relationship to existing and proposed uses on adjacent property;
- (6) The development will preserve and be sensitive to the natural characteristics of the site in a manner that complies with the applicable regulations outlined in this code;
- (7) Adequate provision is made for safe and efficient pedestrian and vehicular circulation within the site and to adjacent property;
- (8) The development will provide adequate lighting for safe and convenient use of the streets, walkways, driveways, and parking areas;
- (9) Upon review and recommendation of the Code Administrator, points of ingress/egress to the development shall be controlled and designed in such a manner as to minimize conflicts with adjacent properties and developments;
- (10) Adequate provision is made for emergency vehicle access and circulation; and
- (11) If the project is to be carried out in progressive stages, each stage shall be so planned that the foregoing criteria are complied with after each stage.

Following its review, the Commission will decide whether to approve, approve with conditions, or deny the application. If the Commission chooses to deny the application, it must cite the specific review criteria in Section 1214.06 (d) and explain why the application does not satisfy the criteria.

Potential Motion: A motion should be made favorably. At least four “yes” votes are required to approve or four “no” votes to deny the application. The language below is provided as a guide and is not meant to suggest any particular action by the Planning Commission.



Planning Commission  
Case No. CPC-25-11  
Ford Motor Company OHAP  
Site Plan Approval  
July 30, 2025  
Page 6 of 6

I move to approve Case No. CPC-25-11, the Ford Motor Company Ohio Assembly Plant Site Plan for the Construction of a New Vehicle Test Track and Installation of a New Stormwater Retention Basin, as submitted

Or subject to the condition that

---

---

---

---

## **SUBSEQUENT ACTION**

Based on approval from the Planning Commission, this plan may proceed to the Building Department for permit submission. If approved outright, no further action is needed. If approved with conditions, all specified conditions must be satisfactorily addressed before proceeding.

## **ATTACHMENTS**

- Planning Commission Application

# LEGEND

New Test Track

Sheffield Lake

Avon Lake

New Retention Pond

Sheffield Village



Site: Ford OHAP (Avon Lake)

Survey: Mar 27, 2025

File created: Jun 11, 2025



## CPC-25-11

### Planning Commission Application

Status: Active

Submitted On: 6/23/2025





### Primary Location

650 MILLER RD  
AVON LAKE, OH 44012

### Owner

FORD MOTOR COMPANY  
650 MILLER RD AVON LAKE, OH 44012

### Applicant

 Patrick Carney  
 419-260-5701  
 patrick.carney@rlgbuilds.com  
 6494 Latcha Rd.  
Walbridge, OH 43465

---

## Property or Parcel Information

### Zoning Classification

I-2

### Present Use\*

Industrial

### Type of Request\*

Site Plan

### General Description of Project\*

Production vehicle test track installation and Stormsewer and retention pond installation.

### Have you had your meeting with the Development Review Committee?\*

Yes

---

## Applicant Information

**Applicant** is the Property Owner or Property Owner's Designee.

**Project Manager** will be the person working closest with the plans and will be the main point of contact for the Planning Department's questions.

### Applicant Role\*

Developer

### Applicant Name\*

Rudolph Libbe Inc.

### Address\*

6494 Latcha Rd.

### City\*

Walbridge

### State\*

OH

### Zip\*

43465

### Phone\*

419.260.5701

### Email\*

Patrick.Carney@rlgbuilds.com

**Project Manager**

Patrick Carney

**Project Manager Phone**

**Project Manager Email**

---

## Property Owner Information

**Name\***

Ford Motor Company

**Address\***

650 Miller Rd.

**City\***

Avon Lake

**State\***

OH

**Zip\***

44012

**Phone\***

NA

---

## Signature

**Applicant Signature\***

✔ Patrick Carney  
Jun 23, 2025

June 23, 2025

Ford OHAP- Clearvision Track

To Whom it May Concern,

We are pleased to submit for your consideration our project narrative for the construction of the Ford OHAP Clearvision Track project.

**Sustainability Narrative:**

- Provide engineering and design for the test track and necessary civil and electrical work.
- Installation of a 30' wide x 808' long concrete track.
- Demo of existing concrete and light poles/bases.
- Site work and grading as necessary.
- Stormwater structures and piping including new headwalls and catch basin.
- Electrical:
  - Installation of new lighting on 20' poles. Lighting to be defused/directed to the test track and limit surrounding light spill as much as possible.
  - Existing electrical feeds to be reworked.
- Striping per FMC standards
- Installation of test track signage as required.
- Installation approx.. 300' of standard, non-ODOT jersey barriers to segregate the test track from other pavement areas.

Thank you for the opportunity to present this project for your consideration. If there are any questions, please feel free to contact me on my mobile at (419) 260-5701

Sincerely,



Patrick Carney

**Project Manager**  
**Rudolph/Libbe Inc.**

June 23, 2025

Ford OHAP- Clearvision Track Sustainability Statement

To Whom it May Concern,

We are pleased to submit for your consideration our sustainability statement for the Ford OHAP Clearvision Test Track project.

**Sustainability Narrative:**

- This project will incorporate stormwater mitigation through new structures and sewer pipe to redirect existing swales within the area of modification.
- The new track lighting will be accomplished with LED fixtures.
- No other sustainability measures are planned or deemed applicable for this project.

Thank you for the opportunity to present this project for your consideration. If there are any questions, please feel free to contact me on my mobile at (419) 260-5701

Sincerely,



Patrick Carney

**Project Manager**  
**Rudolph/Libbe Inc.**

June 23, 2025

Ford OHAP- East Stormwater Management

To Whom it May Concern,

We are pleased to submit for your consideration our project narrative for the construction of the Ford OHAP East Stormwater Management project.

**Sustainability Narrative:**

- Provide engineering and design for a new retention pond, underground stormwater sewers and roof drain reroute.
- Installation of an approx. 40,000 SF retention pond with approx. 1400 LF of stormwater piping, catch basins, manholes, head wall and overflow structure.
- Installation of new fencing and guardrails
- Removal of electrical and vegetation as necessary

Thank you for the opportunity to present this project for your consideration. If there are any questions, please feel free to contact me on my mobile at (419) 260-5701

Sincerely,



Patrick Carney

**Project Manager**  
**Rudolph/Libbe Inc.**

June 23, 2025

Ford OHAP- East Stormwater Management Sustainability Statement

To Whom it May Concern,

We are pleased to submit for your consideration our sustainability statement for the Ford OHAP East Stormwater Management project.

**Sustainability Narrative:**

- This project will redirect roof drains to new underground stormwater sewers and hold water in the new retention pond lessening the burden on the facility and city stormwater sewer system.
- In conjunction with another project in the planning stages, the retention pond would have a Biomatrix floating wetland installed.
- No other sustainability measures are planned or deemed applicable for this project.

Thank you for the opportunity to present this project for your consideration. If there are any questions, please feel free to contact me on my mobile at (419) 260-5701

Sincerely,



Patrick Carney

**Project Manager**  
**Rudolph/Libbe Inc.**

# SITE PLAN - CLEAR VISION TEST TRACK

## FORD OHIO ASSEMBLY PLANT

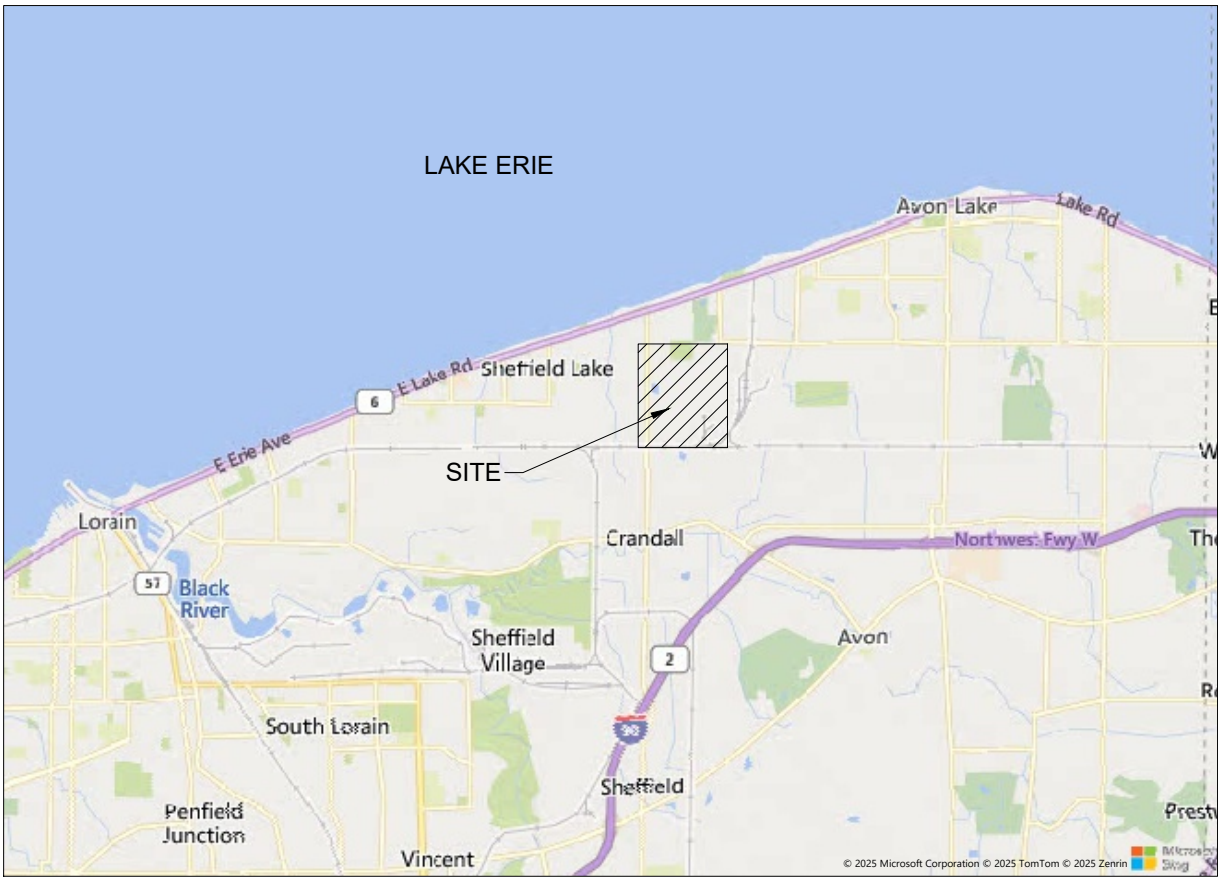
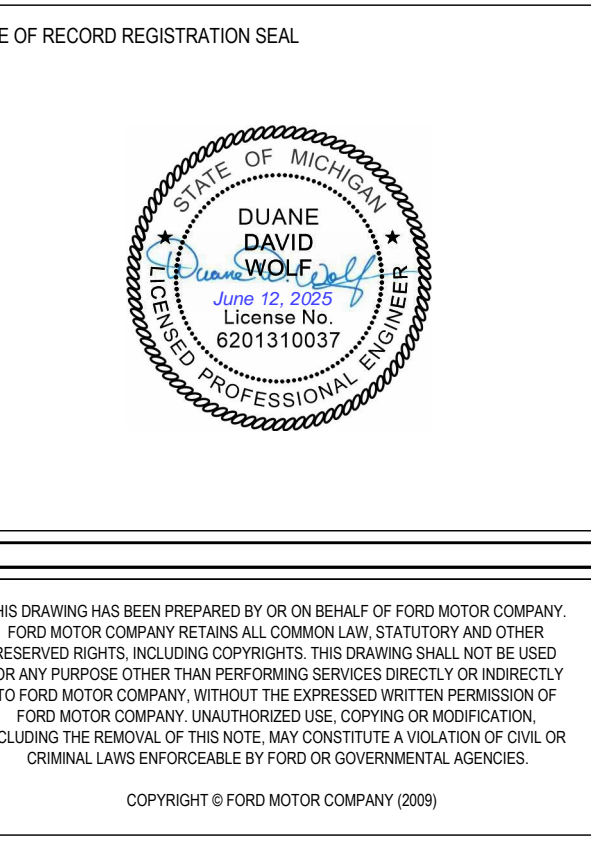
### SHEFFIELD VILLAGE, SHEFFIELD LAKE, AVON LAKE, LORAIN COUNTY, OHIO



OHIO ASSEMBLY PLANT  
650 MILLER ROAD  
AVON LAKE, OH

Ford OHAP  
Clear Vision Test Track  
Civil Sitework

SHEFFIELD / AVON LAKE LORAIN OH  
FORD LAND DOCUMENT PROJECT NUMBER: 3313-004



LOCATION MAP  
SCALE: NONE

CIVIL-SITE DRAWING INDEX			
SHEET NUMBER	SHEET NAME	CURRENT DESCRIPTION	DATE
SP-000	SITE COVER SHEET	SITE PLAN APPROVAL	2025-06-12
SP-100	SITE LAYOUT, GRADING AND UTILITY PLAN	SITE PLAN APPROVAL	2025-06-12

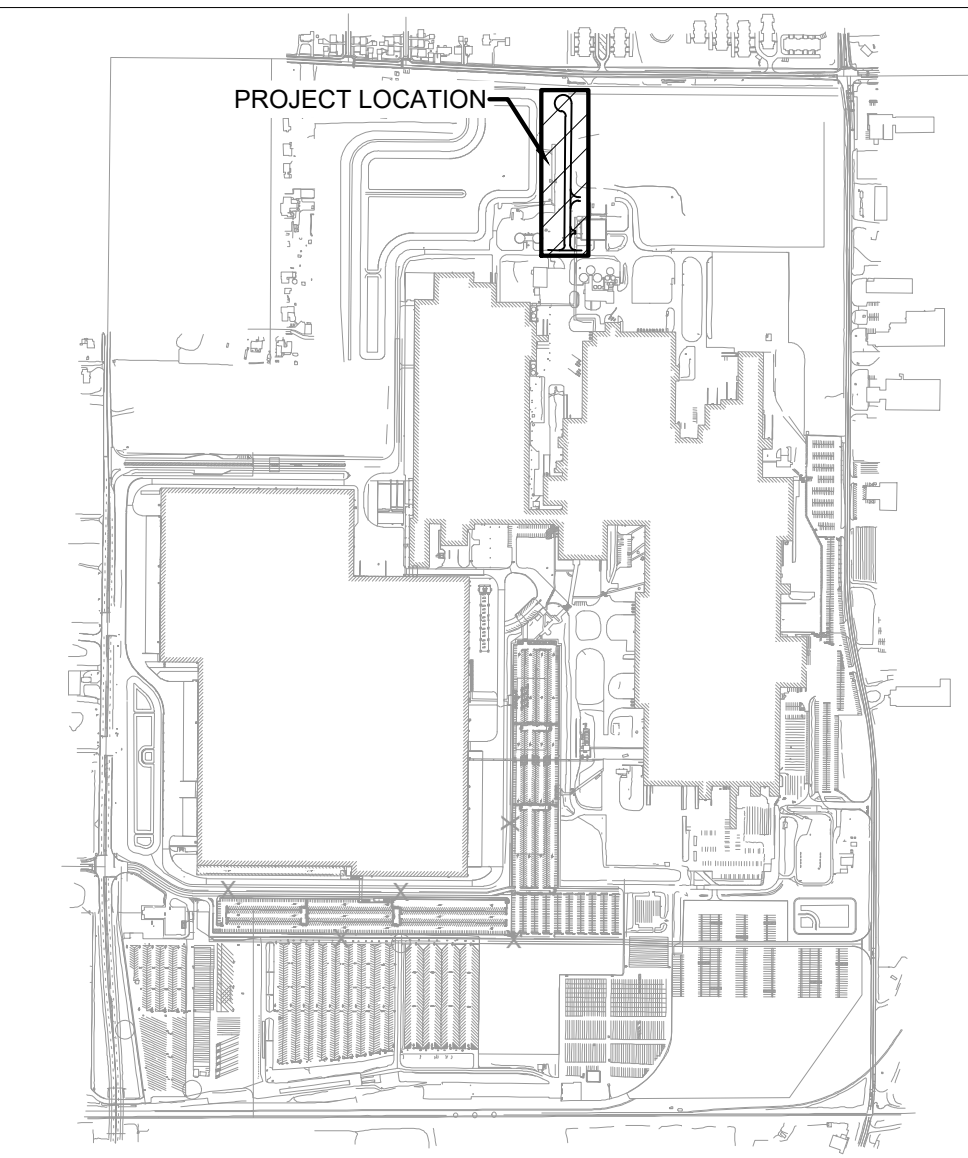
GEOTECHNICAL ENGINEER  
PROFESSIONAL SERVICE INDUSTRIES, INC.  
5555 CANAL ROAD  
CLEVELAND, OH 44125  
P: 216.447.1335

SURVEYOR  
GARCIA SURVEYORS, INC.  
1720 INDIAN WOOD CIRCLE, SUITE E  
MAUMEE, OH 43537  
P: 419.877.0400

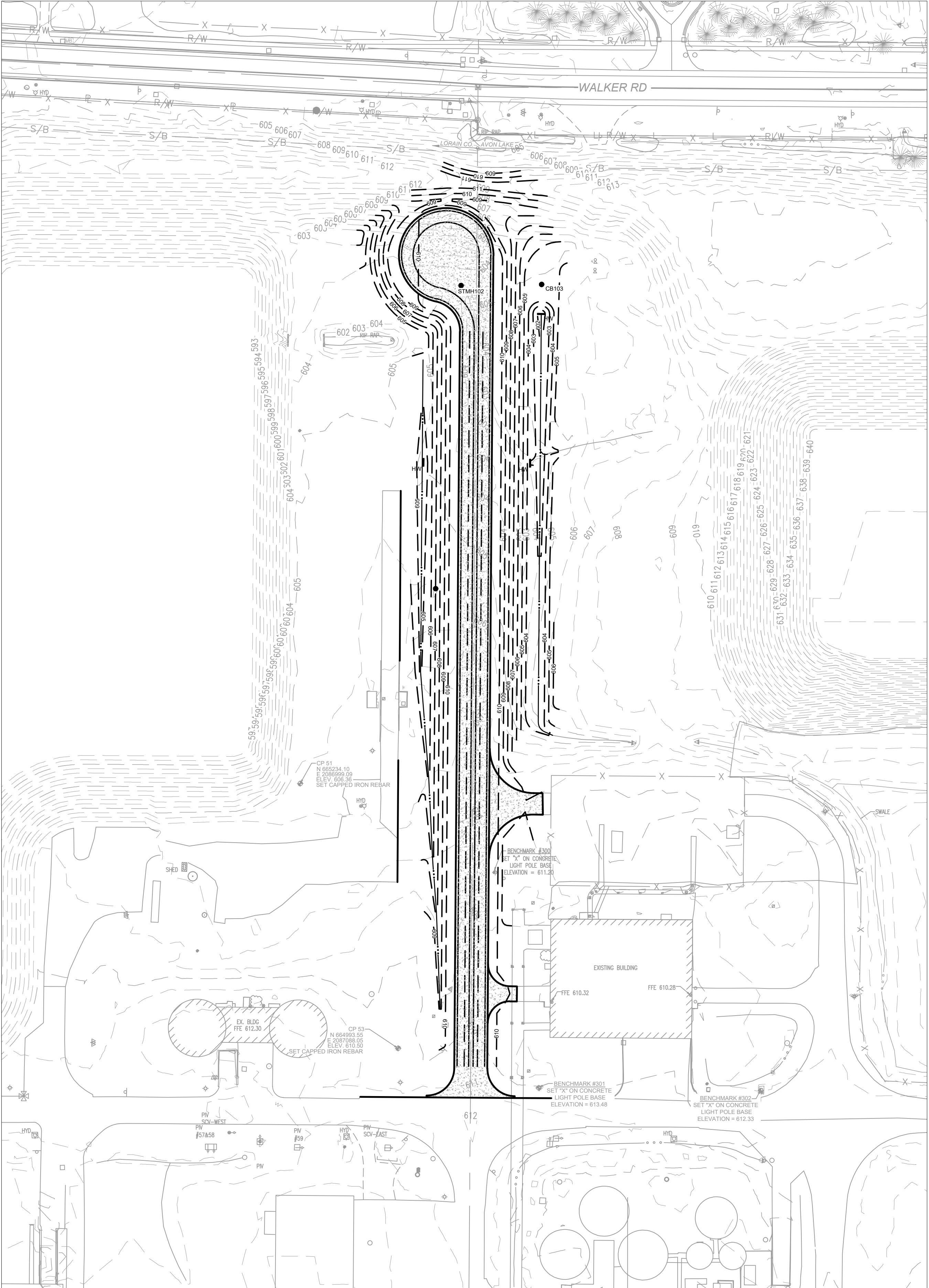
VERTICAL DATUM:  
VERTICAL DATUM IS BASED ON GEOID 12A AND '88 NAVD.

HORIZONTAL DATUM:  
HORIZONTAL DATUM IS BASED ON OHIO STATE PLANE, NORTH ZONE (3401), GR80, NAD83 (2011) AND U.S. SURVEY FEET.

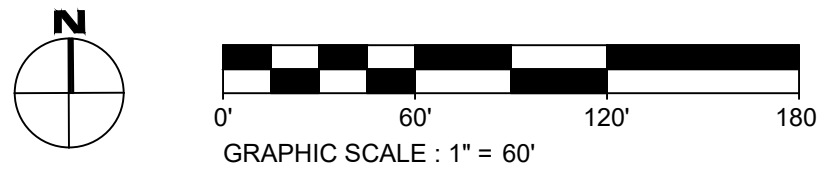
UTILITIES NOTE:  
THE UNDERGROUND UTILITIES SHOWN ON THIS SURVEY HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION, MARKING PROVIDED BY THE UTILITY COMPANIES AND/OR EXISTING DRAWINGS OBTAINED FROM UTILITY COMPANIES. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE OF ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN FACT THE EXACT LOCATION INDICATED, ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM AVAILABLE INFORMATION.



SITE KEY PLAN  
SCALE: 1"=1000'



SITE PLAN  
CLEAR VISION TEST TRACK  
SCALE: 1" = 60'

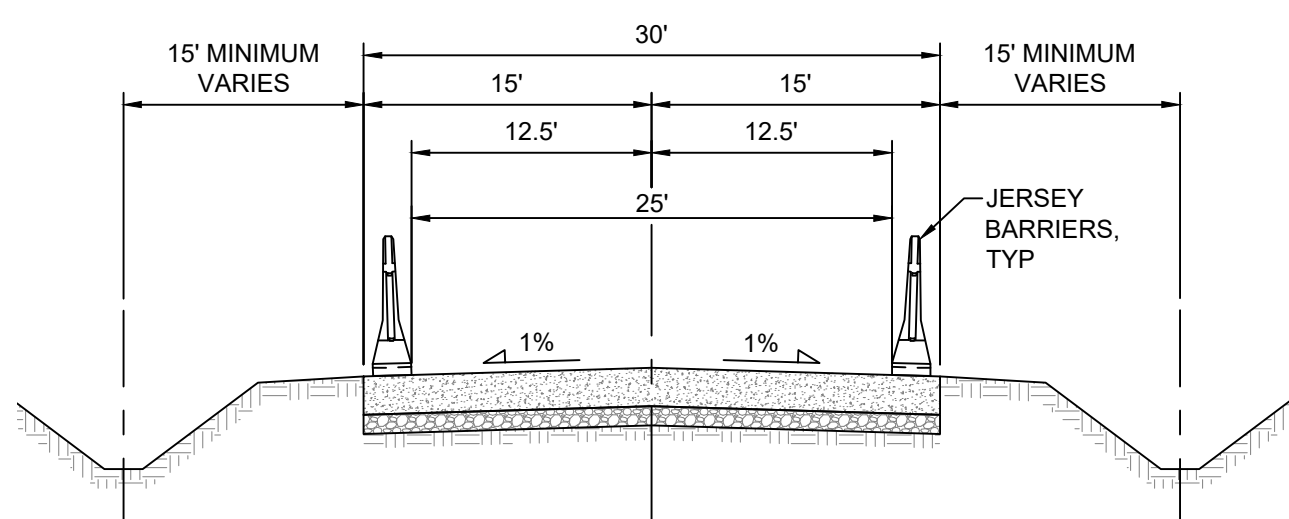
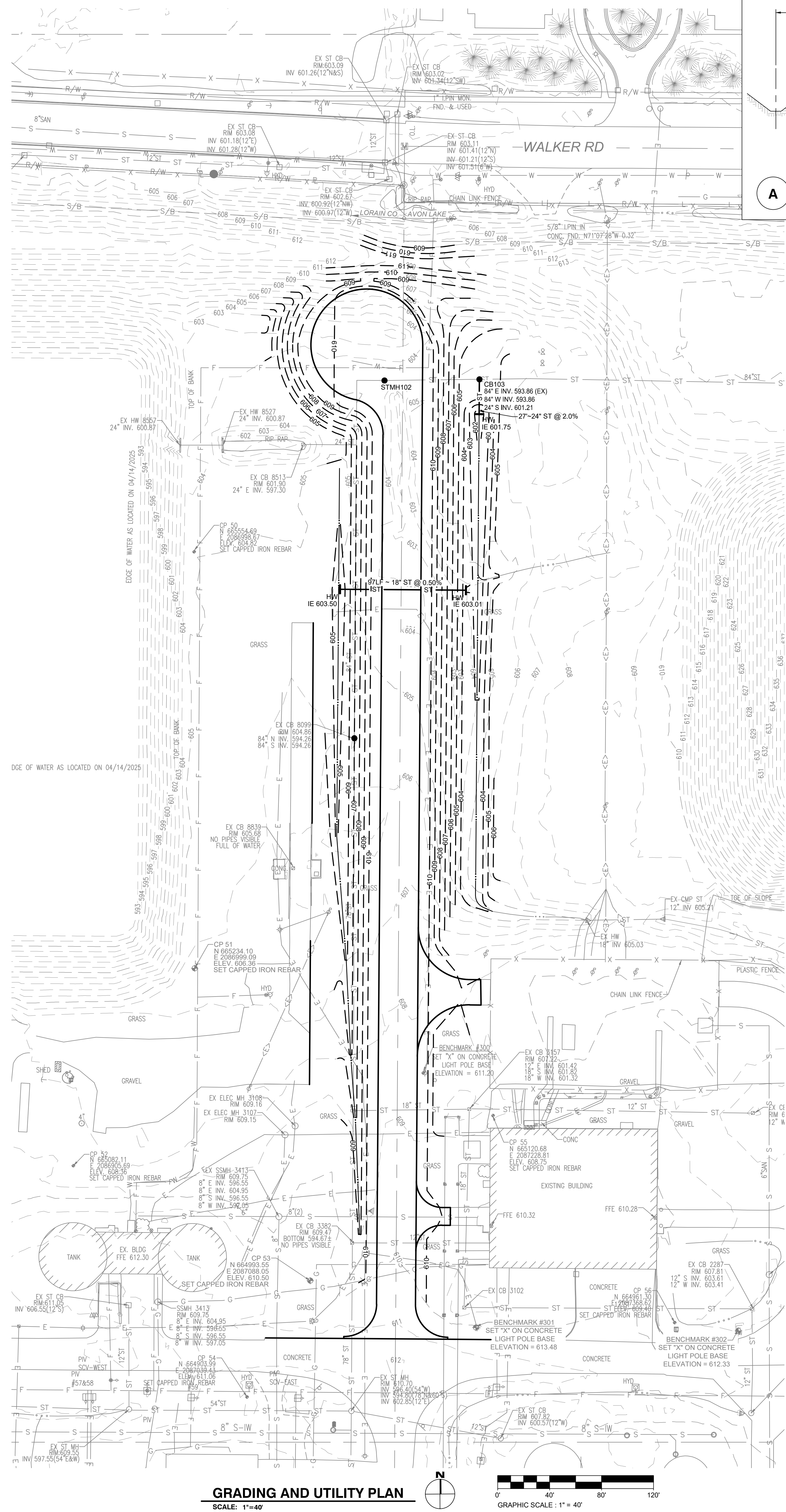
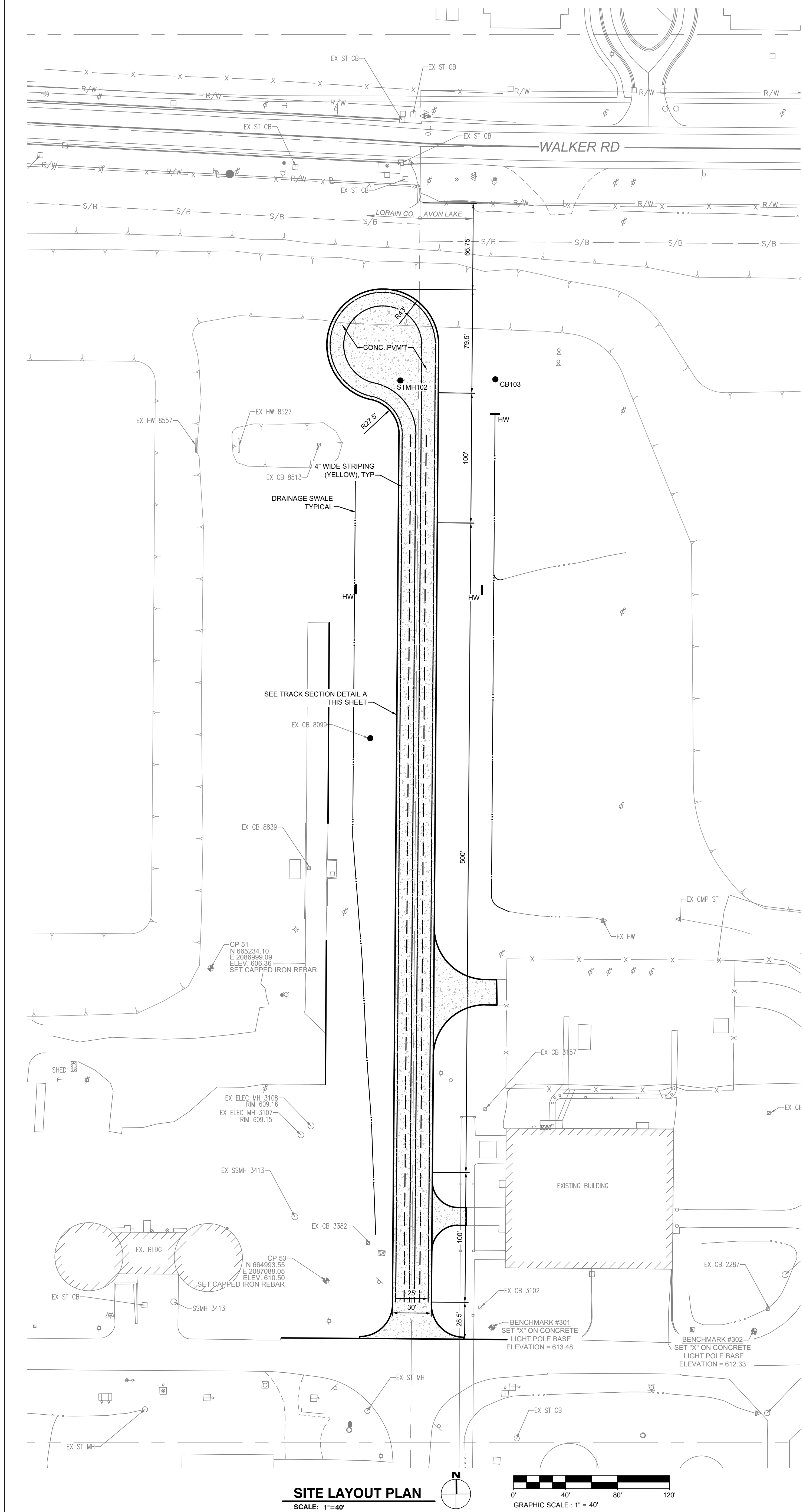


SURVEY CONTROL POINTS & BENCH MARK				
NO.	DESCRIPTION	NORTHING	EASTING	ELEVATION
CP#51	SET CAPPED IRON REBAR	665234.10	2086999.09	606.36
CP#53	SET CAPPED IRON REBAR	664993.55	2087088.05	610.50
CP#55	SET CAPPED IRON REBAR	665120.68	2087228.81	608.75
BM#300	SET "X" ON CONC. LIGHT POLE BASE			611.20
BM#301	SET "X" ON CONC. LIGHT POLE BASE			613.48
BM#302	SET "X" ON CONC. LIGHT POLE BASE			612.33

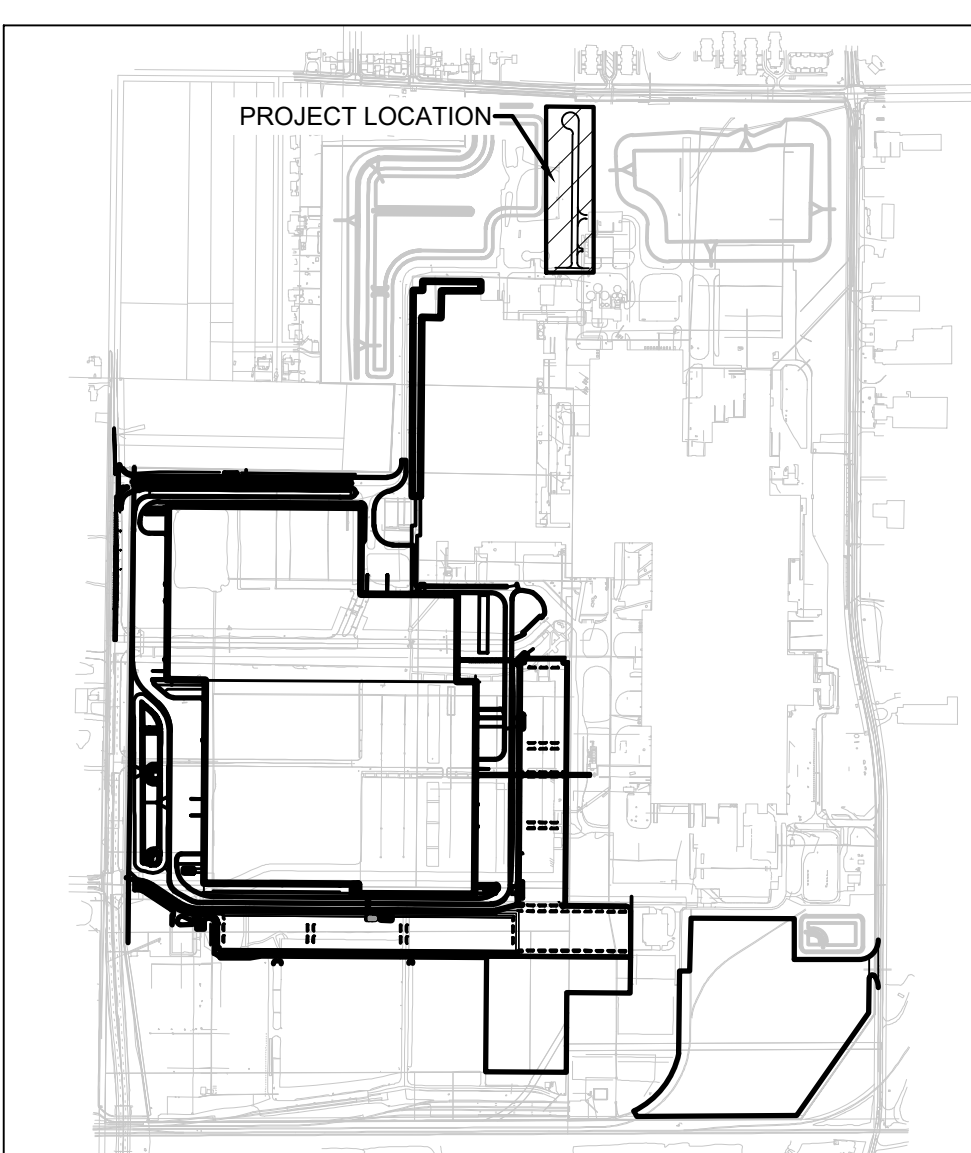
COORDINATE SYSTEM: OH83-NF (NAD83 Ohio State Planes, North Zone, US Foot)



A/E TEAM INFORMATION	
IN CHARGE:	M. STARR
DRAWN BY:	D. GREENHILL
DESIGNED BY:	D. WOLF
CHECKED BY:	C. HARTMAN
APPROVED BY:	M. STARR
A/E PROJECT NUMBER:	022-00543-04
SHEET TITLE	
SITE COVER SHEET	
SHEET NUMBER	3313-004
SP-000	



**A CLEARVISION AUDIT TRACK SECTION**  
SCALE: NONE



**FORD LAND**  
330 TOWN CENTER DRIVE, SUITE 1100  
DEARBORN, MICHIGAN 48126-2738  
USA

**OHIO ASSEMBLY PLANT**  
650 MILLER ROAD  
AVON LAKE, OH

Ford OHAP  
Clear Vision Test Track  
Civil Sitework

SHEFFIELD / AVON LAKE    LORAIN    OH

FORD LAND DOCUMENT PROJECT NUMBER:    3313-004

AE OF RECORD LOGO

**RUDOLPH  
LIBBE  
GROUP**

**SSOE**

PROJECT NUMBER: 022-00543-00

AE OF RECORD REGISTRATION SEAL

**DUANE  
DAVID  
WOLF**

License No. 620131003

PROFESSIONAL ENGINEER

THIS DRAWING HAS BEEN PREPARED BY OR ON BEHALF OF FORD MOTOR COMPANY. FORD MOTOR COMPANY RETAINS ALL COMMON LAW, STATUTORY AND OTHER RESERVED RIGHTS, INCLUDING COPYRIGHTS. THIS DRAWING SHALL NOT BE USED FOR ANY PURPOSE OTHER THAN THE PERFORMANCE OF SERVICES DIRECTLY ON BEHALF OF FORD MOTOR COMPANY WITHOUT THE EXPRESS WRITTEN PERMISSION OF FORD MOTOR COMPANY. ANY UNAUTHORIZED USE, COPYING OR REPRODUCTION, INCLUDING THE REPRODUCTION OF THIS NOTE, MAY CONSTITUTE A VIOLATION OF CIVIL OR CRIMINAL LAWS ENFORCEABLE BY FORD OR GOVERNMENTAL AGENCIES.

COPYRIGHT © FORD MOTOR COMPANY (2009)

DATE	SITE PLAN APPROVAL	ISSUED FOR
2025-06-12		

AE TEAM INFORMATION

IN CHARGE:	M. STARR
DRAWN BY:	D. GREENHILL
DESIGNED BY:	D. WOLF
CHECKED BY:	C. HARTMAN
APPROVED BY:	M. STARR

AE PROJECT NUMBER: 022-00543-00

SHEET TITLE

**SITE LAYOUT, GRADING AND UTILITY PLAN**

SHEET NUMBER    3313-004

**SP-100**



LOCATION MAP



PROJECT DESCRIPTION

RETENTION POND DESIGN AND INTERCONNECTING  
STORM SEWER REPLACEMENT PROJECT

FORD LAND DOCUMENT PROJECT NUMBER

3313-000

CITY SUBMITTALS

LIST OF CITY SUBMITTALS

LIST OF DEFERRED CITY SUBMITTALS

PROJECT NAME AND ADDRESS

OHIO ASSEMBLY PLANT  
AVON LAKE  
650 MILLER ROAD AVON LAKE, OH 44012  
STORMWATER  
MANAGEMENT  
DESIGN

PROJECT TEAM

FORD KEY CONTACTS

FORD LAND PROJECT MANAGER:

"Ford Land PJM Name"  
"Ford Land PJM Phone Number"

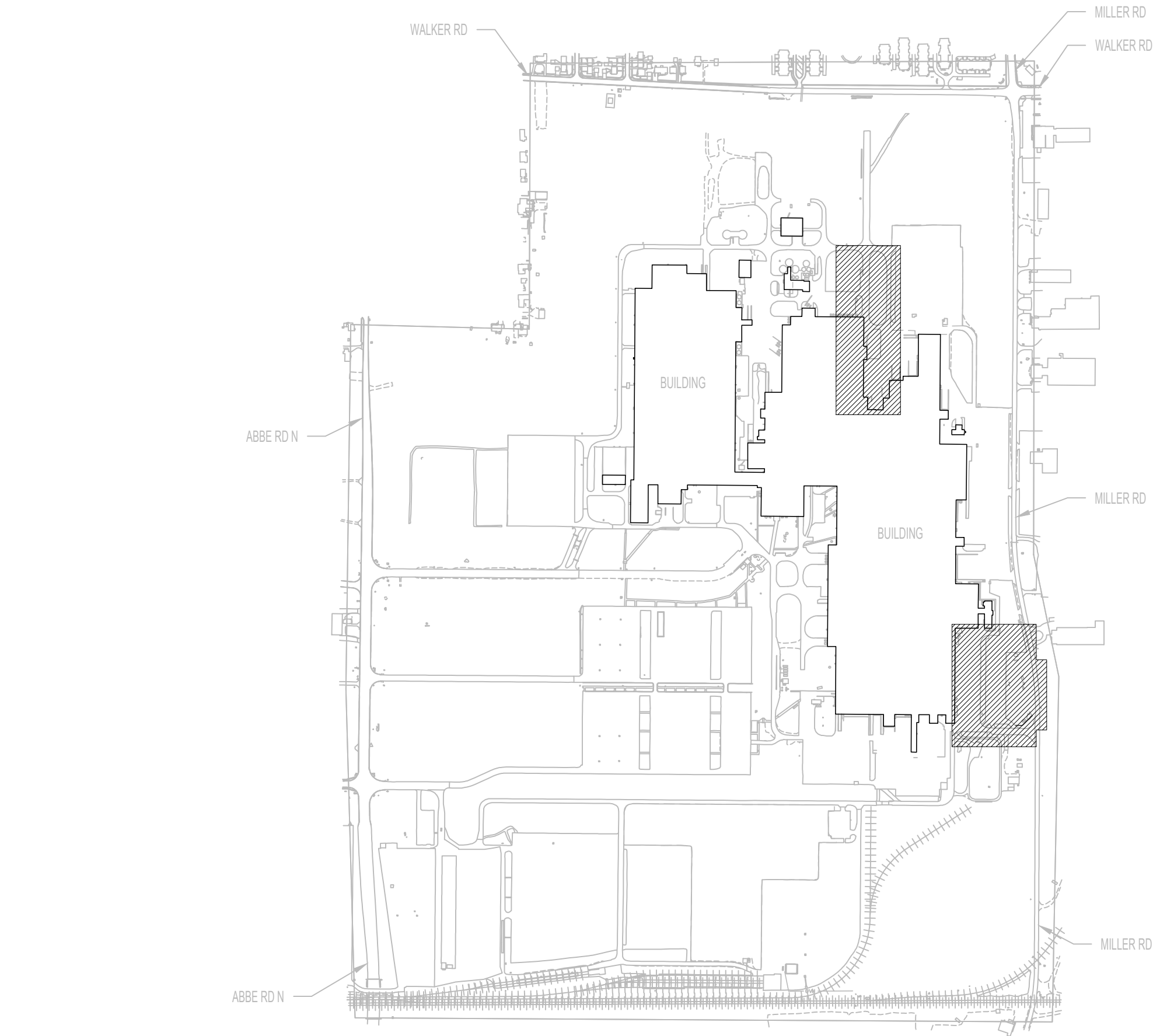
FORD LAND WORKPLACE STRATEGY MANAGER:

"Ford Land WSM Name"  
"Ford Land WSM Phone Number"

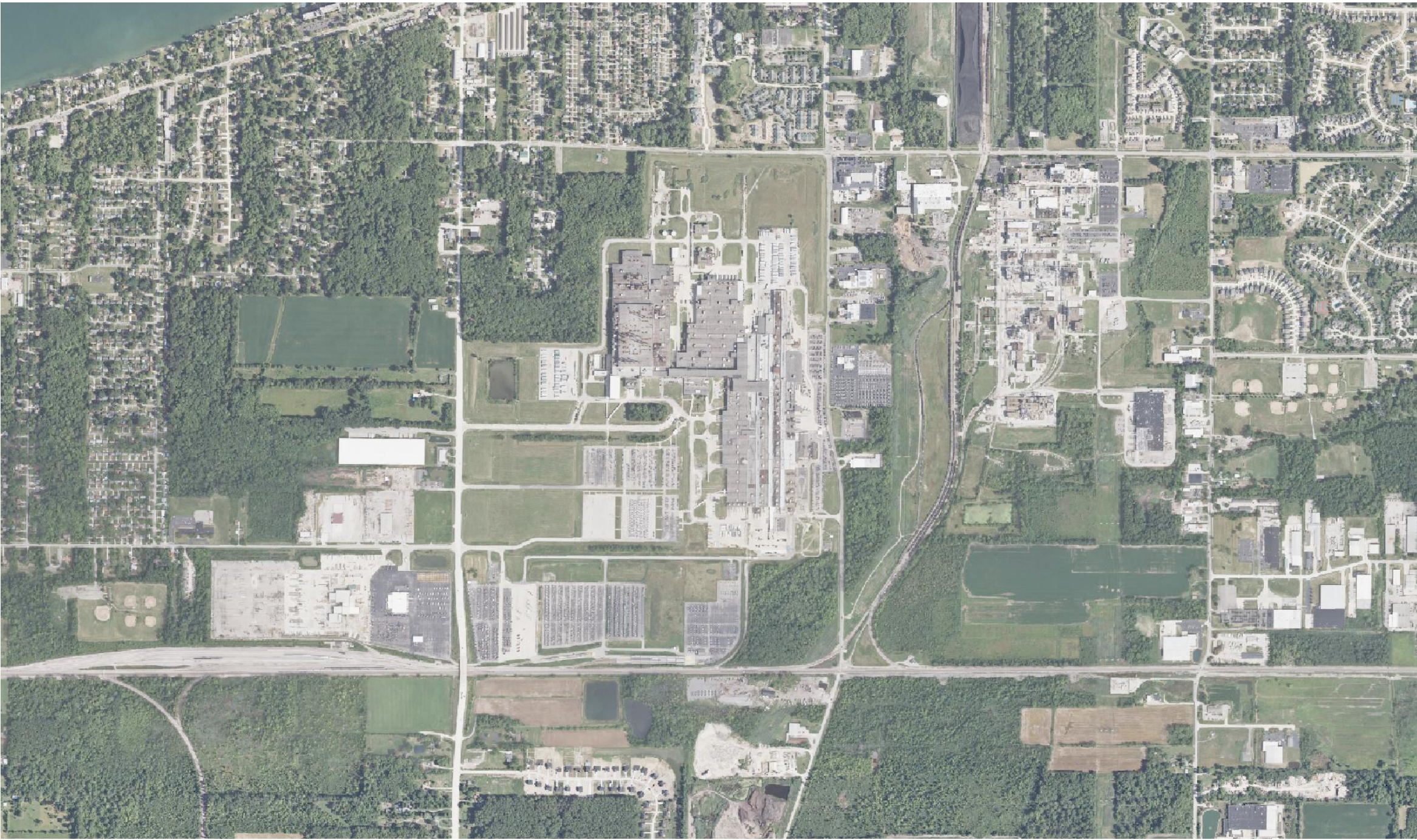
FORD LAND DCT PLANNING MANAGER:

"Ford Land DCT PJM Name"  
"Ford Land DCT PJM Phone Number"

KEY PLAN



PROJECT IMAGE



A/E PROJECT INFORMATION

30203632  
NOT FOR CONSTRUCTION  
APRIL 4TH, 2024





## EXISTING SYMBOLS

## PROPOSED SYMBOLS

CONTROL POINTS				
Point #	ELEVATION	NORTHING	EASTING	DESCRIPTION
200	609.80	664357.30	2088638.64	CNPT MAGS
300	607.32	664942.93	2088636.94	CNPT IPINS
301	612.41	663804.43	2088594.98	CNPT MAGS
302	620.02	663143.20	2088659.36	CNPT MAGS
303	623.83	662489.17	2088746.23	CNPT IPINS
10975	620.28	662099.91	2088105.67	CNPT MAGS
10976	620.34	662104.97	2088167.42	CNPT MAGS
31184	613.89	663815.62	20883359.99	CNPT MAGS
31186	617.24	663306.46	2088403.17	CNPT MAGS
31225	620.01	663048.95	2088466.74	CNPT MAGS
31400	621.30	662969.31	2088451.37	CNPT MAGS
40010	622.12	662625.90	2088529.72	CNPT MAGS
40396	619.23	663176.81	2088561.82	CNPT MAGS
41862	620.46	662101.93	2088268.10	CNPT MAGS
41863	621.50	662373.81	2088373.26	CNPT MAGS
41864	622.04	662632.83	2088518.17	CNPT MAGS
42584	621.86	662839.29	2088463.25	CNPT MAGS

## ABBREVIATIONS

  
**FORDLAND**

17000 ROTUNDA DR, 1ST FLOOR SOUTH  
DEARBORN, MICHIGAN 48120-1168  
USA



OHIO ASSEMBLY PLANT  
AVON LAKE, OH 44012  
650 MILLER ROAD  
  
STORMWATER  
MANAGEMENT  
DESIGN

AVON LAKE    LORAIN COUNTY    OHIO

FORD LAND DOCUMENT PROJECT NUMBER: 3313-000

## AIE OF RECORD LOG



LEGAL ENTITY:  
ARCADIS U.S., INC

A/E OF RECORD REGISTRATION SEAL

PRELIMINARY  
NOT FOR CONSTRUCTION

THIS DRAWING HAS BEEN PREPARED BY OR ON BEHALF OF FORD MOTOR COMPANY. FORD MOTOR COMPANY RETAINS ALL COMMON LAW, STATUTORY AND OTHER RESERVED RIGHTS INCLUDING COPYRIGHTS. THIS DRAWING SHALL NOT BE USED FOR ANY PURPOSE OTHER THAN PERFORMING SERVICES DIRECTLY OR INDIRECTLY TO FORD MOTOR COMPANY, WITHOUT THE EXPRESSED WRITTEN PERMISSION OF FORD MOTOR COMPANY. UNAUTHORIZED USE, COPYING OR MODIFICATION, INCLUDING THE REMOVAL OF THIS NOTICE, MAY CONSTITUTE A VIOLATION OF CIVIL OR CRIMINAL LAWS ENFORCEABLE BY FORD OR GOVERNMENTAL AGENCIES.

**COPYRIGHT © FORD MOTOR COMPANY (2005)**

[illegible]

A/E TEAM INFORMATION

IN CHARGE: F. LAST NAME

DRAWN BY: M. WELSHANS

DESIGNED BY: K. ASCHMEIER

CHECKED BY: K. ASCHMEIER

APPROVED BY: F. LAS

SHEET TITLE

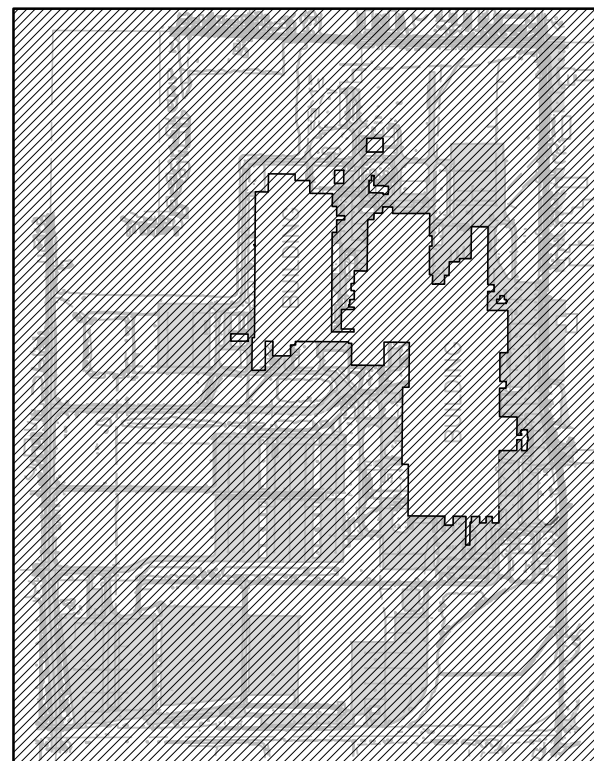
EX. SITE PLAN, ABBREVIATIONS  
LEGEND AND DRAWING INDEX

SHEET NUMBER

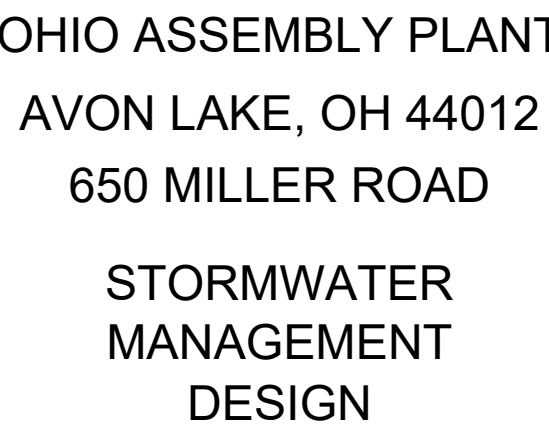
C-001





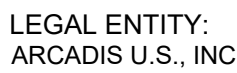


	EXISTING MAJOR TOPOGRAPHIC CONTOURS
	EXISTING MINOR TOPOGRAPHIC CONTOURS
	LIMITS OF EXISTING ASPHALT PAVING
	EXISTING LIMITS OF TREE LINE/ VEGETATION
	EXISTING TREE
	EXISTING STORM PIPE
	EXISTING CHAINLINK FENCE
	EXISTING GAS LINE
	EXISTING GAS LINE
	EXISTING GAURD RAIL
	EXISTING RAILROAD TRACKS
	EXISTING PROPERTY LINE
	EXISTING RIGHT OF WAY



AVON LAKE    LORAIN COUNTY    OHIO

FORD LAND DOCUMENT PROJECT NUMBER: 3313-000



PRELIMINARY  
NOT FOR CONSTRUCTION

THIS DRAWING HAS BEEN PREPARED BY OR ON BEHALF OF FORD MOTOR COMPANY. FORD MOTOR COMPANY RETAINS ALL COMMON LAW, STATUTORY AND OTHER RESERVED RIGHTS INCLUDING COPYRIGHTS. THIS DRAWING SHALL NOT BE USED FOR ANY PURPOSE OTHER THAN PERFORMING SERVICES DIRECTLY OR INDIRECTLY TO FORD MOTOR COMPANY, WITHOUT THE EXPRESSED WRITTEN PERMISSION OF FORD MOTOR COMPANY. UNAUTHORIZED USE, COPYING OR MODIFICATION, INCLUDING THE REMOVAL OF THIS NOTICE, MAY CONSTITUTE A VIOLATION OF CIVIL OR CRIMINAL LAWS ENFORCEABLE BY FORD OR GOVERNMENTAL AGENCIES.

**COPYRIGHT © FORD MOTOR COMPANY (2005)**

[illegible]

#### A/E TEAM INFORMATION

IN CHARGE: F. LAST NAME

DRAWN BY: M. WELSHANS

DESIGNED BY: K. ASCHMEIER

CHECKED BY: K. ASCHMEIER

APPROVED BY: F. LAS

A/E PROJECT NUMBER: 30203632

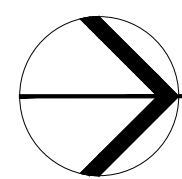
SHEET TITLE OVERALL

OVERALL  
EMERGENCY OFF PLAN

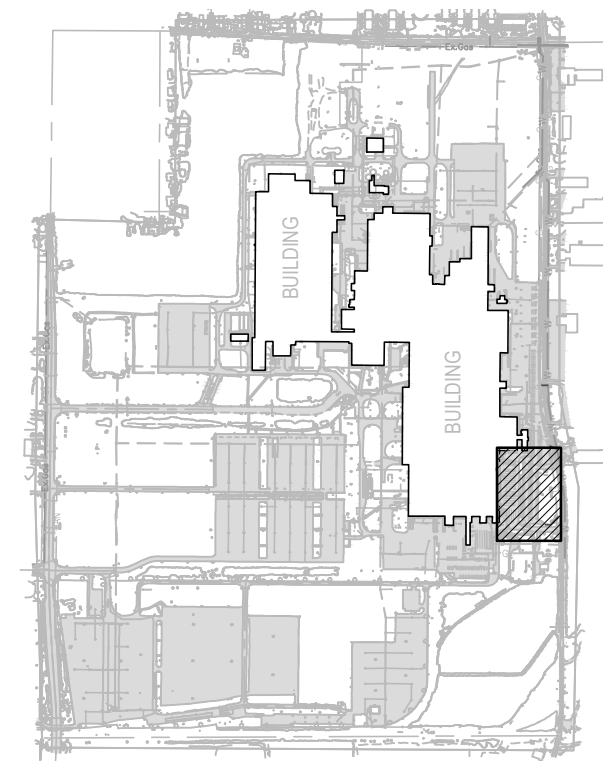
EXISTING SITE PLAN

SHEET NUMBER

C-001



SCALE: 1" = 50'

 **KEY PLAN**

//////.

ITEMS TO BE DEMOLISHED



# STORMWATER MANAGEMENT DESIGN

FORD LAND DOCUMENT PROJECT NUMBER: 3313-000

A/E OF RECORD LOGO



LEGAL ENTITY:  
ARCADIS U.S., INC

PRELIMINARY  
NOT FOR CONSTRUCTION

THIS DRAWING HAS BEEN PREPARED BY OR ON BEHALF OF FORD MOTOR COMPANY. FORD MOTOR COMPANY RETAINS ALL COMMON LAW, STATUTORY AND OTHER RESERVED RIGHTS INCLUDING COPYRIGHTS. THIS DRAWING SHALL NOT BE USED FOR ANY PURPOSE OTHER THAN PERFORMING SERVICES DIRECTLY OR INDIRECTLY TO FORD MOTOR COMPANY, WITHOUT THE EXPRESSED WRITTEN PERMISSION OF FORD MOTOR COMPANY. UNAUTHORIZED USE, COPYING OR MODIFICATION, INCLUDING THE REMOVAL OF THIS NOTICE, MAY CONSTITUTE A VIOLATION OF CIVIL OR CRIMINAL LAWS ENFORCEABLE BY FORD OR GOVERNMENTAL AGENCIES.

COPYRIGHT © FORD MOTOR COMPANY (2005)

[illegible]

A/E TEAM INFORMATION

IN CHARGE: F. LAST NAME

DRAWN BY: M. WELSHANS

DESIGNED BY: K. ASCHMEIER

CHECKED BY: K. ASCHEMEIER

APPROVED BY: F. LAST NAME

SHEET TITLE

ROOF DRAIN SEWER EXISTING  
CONDITIOPNS PLAN

SHEET NUMBER

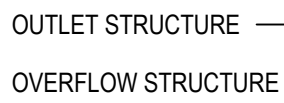
C-004

C:\Users\WELSHANS\ACCDocs\Arcadis ACC US\AUS-30203532-Ford OHAP 2024 Storm Design Management\Project Files\WIP\Civil\3313-000-C-004.dwg

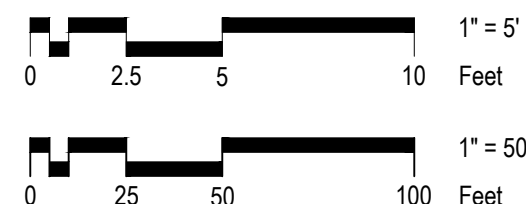
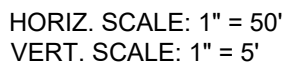
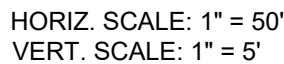
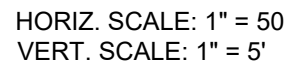
WELSHANS, MATTHEW

SAVE DATE: 4/2/2024 11:47:35 PM

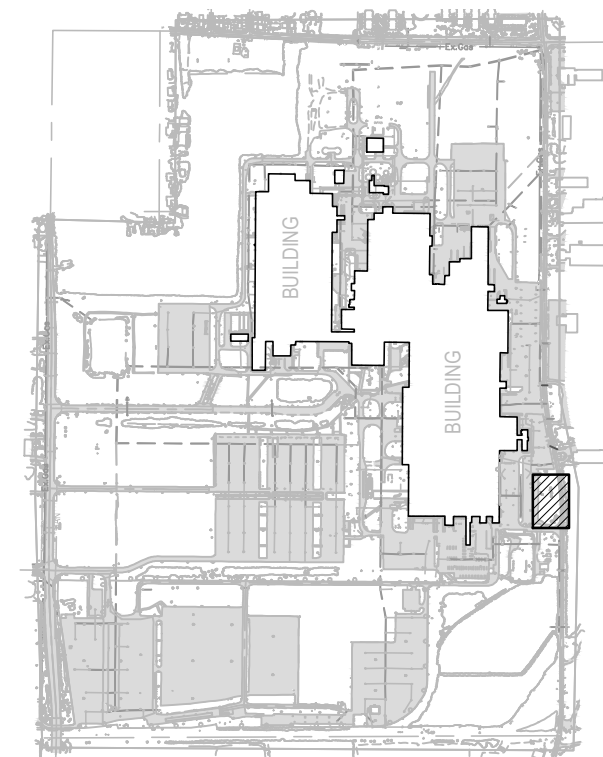
PLOT DATE: 4/4/2024 2:59:28 AM



SCALE: 1" = 50'



C-005



	EXISTING MAJOR TOPOGRAPHIC CONTOURS
	EXISTING MINOR TOPOGRAPHIC CONTOURS
	LIMITS OF EXISTING ASPHALT PAVING
	EXISTING LIMITS OF TREELINE/ VEGETATION
	EXISTING STORM PIPE
	EXISTING CHAINLINK FENCE
	EXISTING GAS LINE
	ITEMS TO BE DEMOLISHED



OHIO ASSEMBLY PLANT  
AVON LAKE, OH 44012  
650 MILLER ROAD  
  
STORMWATER  
MANAGEMENT  
DESIGN

AVON LAKE    LORAIN COUNTY    OHIO

FORD LAND DOCUMENT PROJECT NUMBER: 3313-000

A/E OF RECORD LOGO



LEGAL ENTITY:  
ARCADIS U.S., INC

A/E OF RECORD REGISTRATION SEAL

PRELIMINARY  
NOT FOR CONSTRUCTION

THIS DRAWING HAS BEEN PREPARED BY OR ON BEHALF OF FORD MOTOR COMPANY. FORD MOTOR COMPANY RETAINS ALL COMMON LAW, STATUTORY AND OTHER RESERVED RIGHTS INCLUDING COPYRIGHTS. THIS DRAWING SHALL NOT BE USED FOR ANY PURPOSE OTHER THAN PERFORMING SERVICES DIRECTLY OR INDIRECTLY TO FORD MOTOR COMPANY, WITHOUT THE EXPRESSED WRITTEN PERMISSION OF FORD MOTOR COMPANY. UNAUTHORIZED USE, COPYING OR MODIFICATION, INCLUDING THE REMOVAL OF THIS NOT, MAY CONSTITUTE A VIOLATION OF CIVIL OR CRIMINAL LAWS ENFORCEABLE BY FORD OR GOVERNMENTAL AGENCIES.

COPYRIGHT © FORD MOTOR COMPANY (2005)

[illegible]

A/E TEAM INFORMATION

IN CHARGE: F. LAST NAME

DRAWN BY: M. WELSHANS

DESIGNED BY: P. OTIS

CHECKED BY: P. OTIS

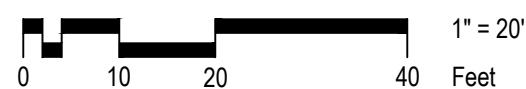
APPROVED BY: \_\_\_\_\_ F. LAST NAME

**SHEET TITLE**

# STORMWATER DETENTION POND EXISTING CONDITIONS PLAN

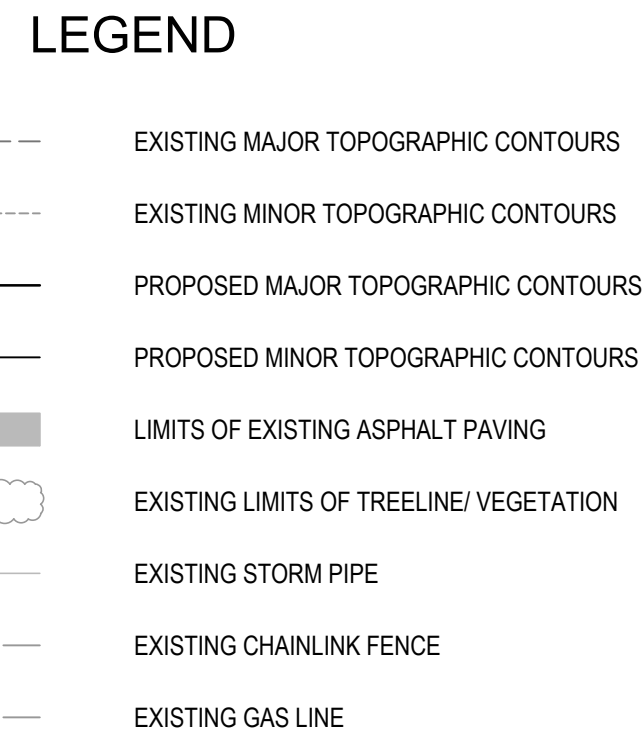
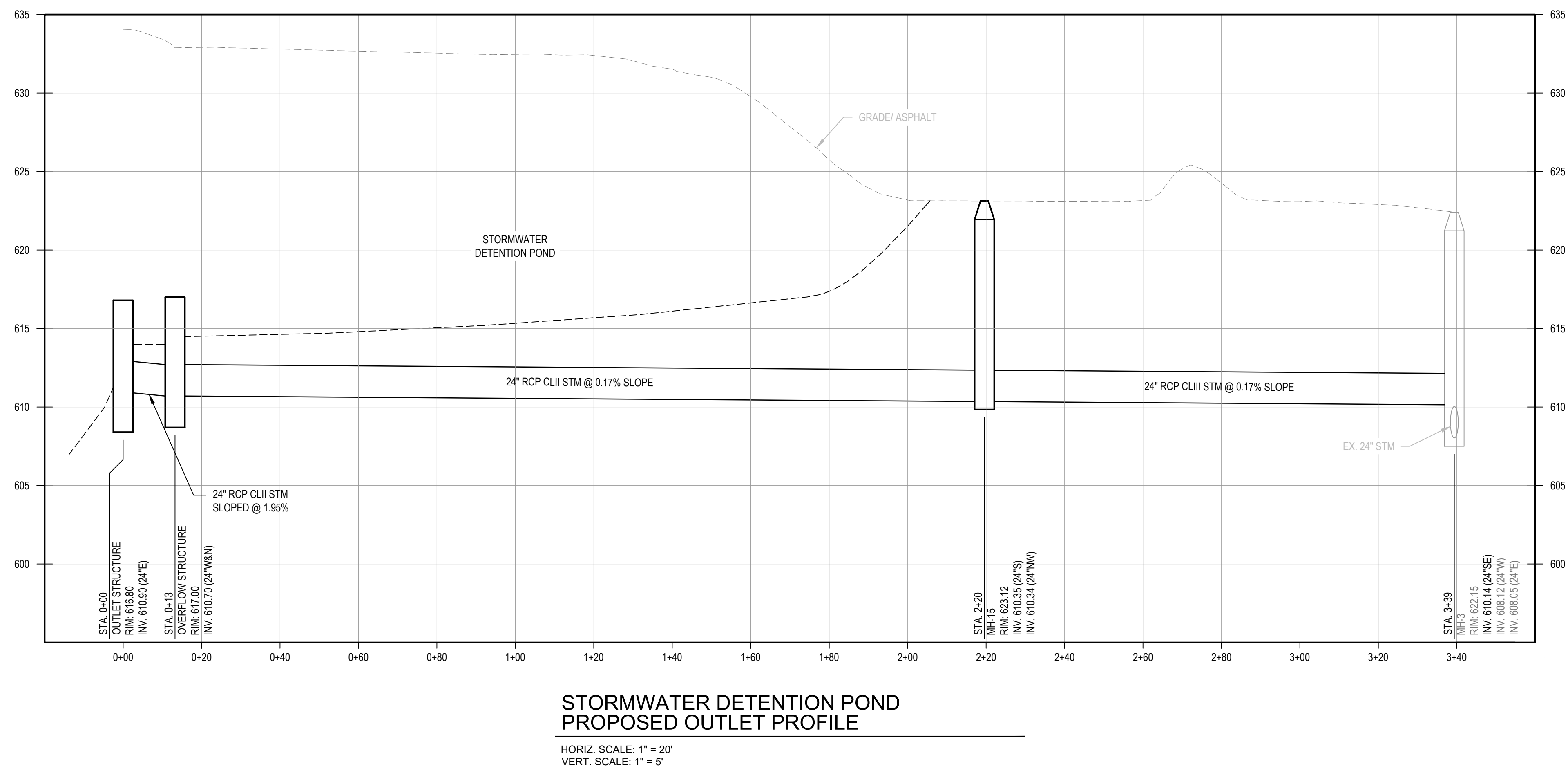
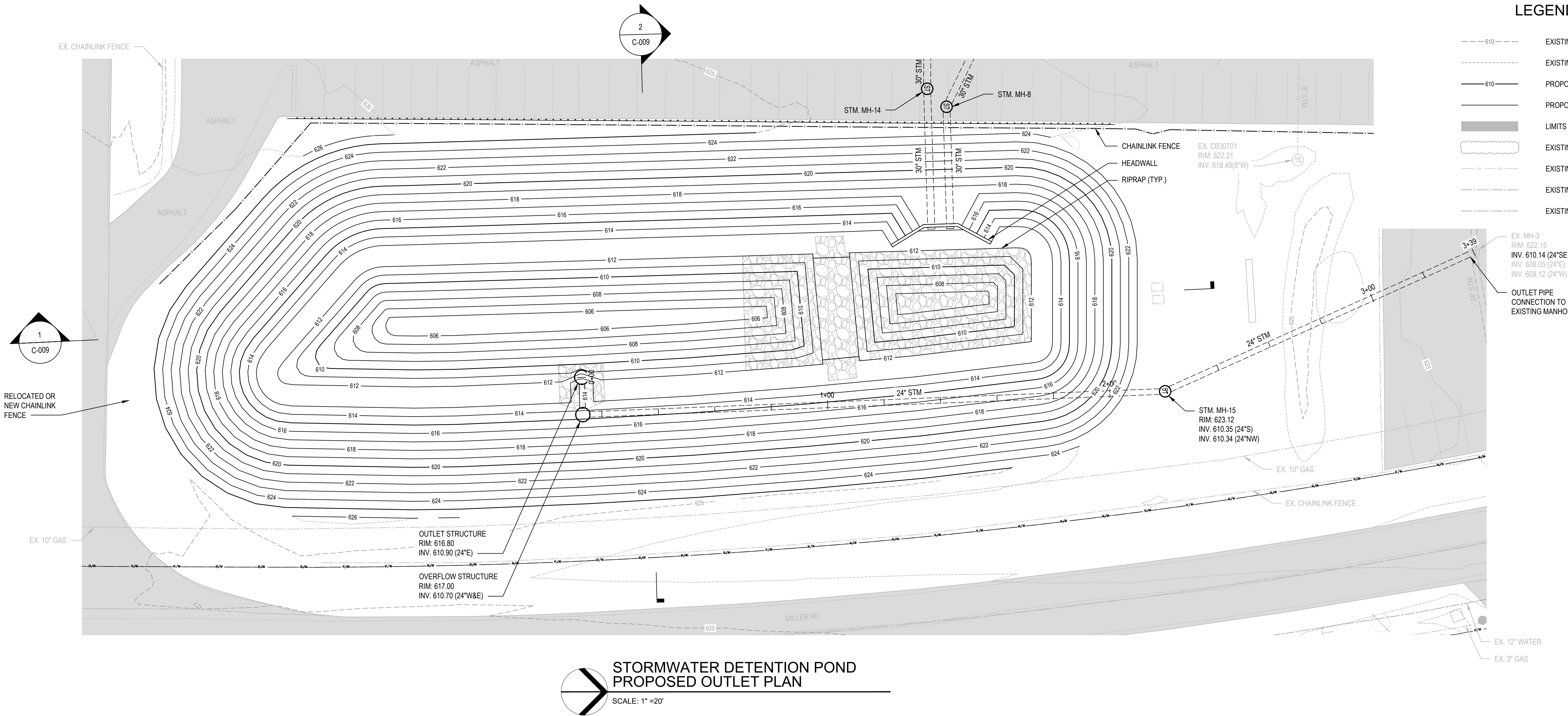
SHEET NUMBER

C-006



1. AREAS OF OF DISTURBANCE TO BE SEEDS  
AS PER FORD LAND SPECIFICATIONS IN ALL  
AREAS EXCEPT FOR WHERE ALTERNATIVE  
COVER IS CALLED OUT ON THE PLAN.

C-007



  
**FORDLAND**  
17000 ROTUNDA DR, 1ST FLOOR SOUTH  
DEARBORN, MICHIGAN 48120-1168  
USA  


OHIO ASSEMBLY PLANT  
AVON LAKE, OH 44012  
650 MILLER ROAD  
  
STORMWATER  
MANAGEMENT  
DESIGN

AVON LAKE    LORAIN COUNTY    OHIO

FORD LAND DOCUMENT PROJECT NUMBER: 3313-000

### A/E OF RECORD LOG



LEGAL ENTITY:  
ARCADIS U.S., INC

A/E OF RECORD REGISTRATION SEAL

PRELIMINARY  
NOT FOR CONSTRUCTION

THIS DRAWING HAS BEEN PREPARED BY OR ON BEHALF OF FORD MOTOR COMPANY. FORD MOTOR COMPANY RETAINS ALL COMMON LAW, STATUTORY AND OTHER RESERVED RIGHTS INCLUDING COPYRIGHTS. THIS DRAWING SHALL NOT BE USED FOR ANY PURPOSE OTHER THAN PERFORMING SERVICES DIRECTLY OR INDIRECTLY TO FORD MOTOR COMPANY, WITHOUT THE EXPRESSED WRITTEN PERMISSION OF FORD MOTOR COMPANY. UNAUTHORIZED USE, COPYING OR MODIFICATION, INCLUDING THE REMOVAL OF THIS NOTICE, MAY CONSTITUTE A VIOLATION OF CIVIL OR CRIMINAL LAWS ENFORCEABLE BY FORD OR GOVERNMENTAL AGENCIES.

COPYRIGHT © FORD MOTOR COMPANY (2009)

[illegible]

#### A/E TEAM INFORMATION

IN CHARGE: F. LAST NAME

DRAWN BY: M. WELSHANS

DESIGNED BY: P. OTIS

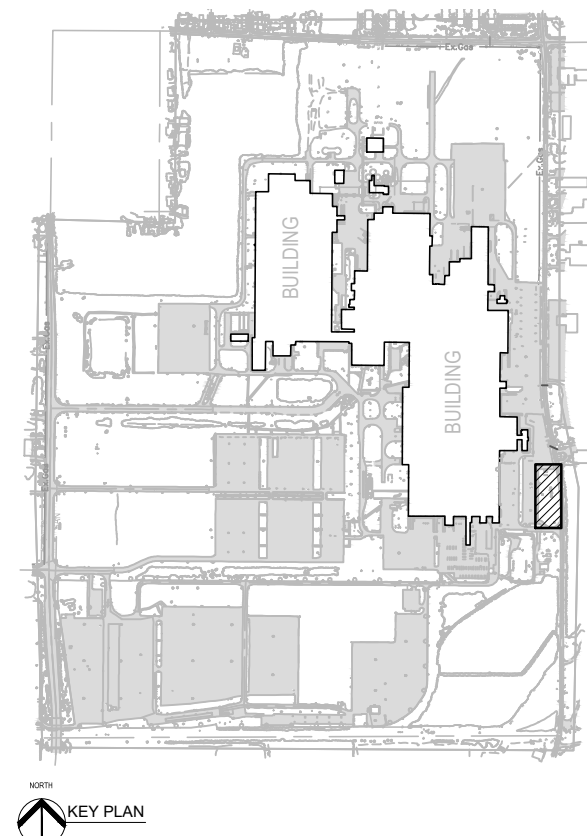
CHECKED BY: P. OTIS

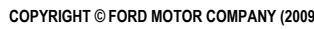
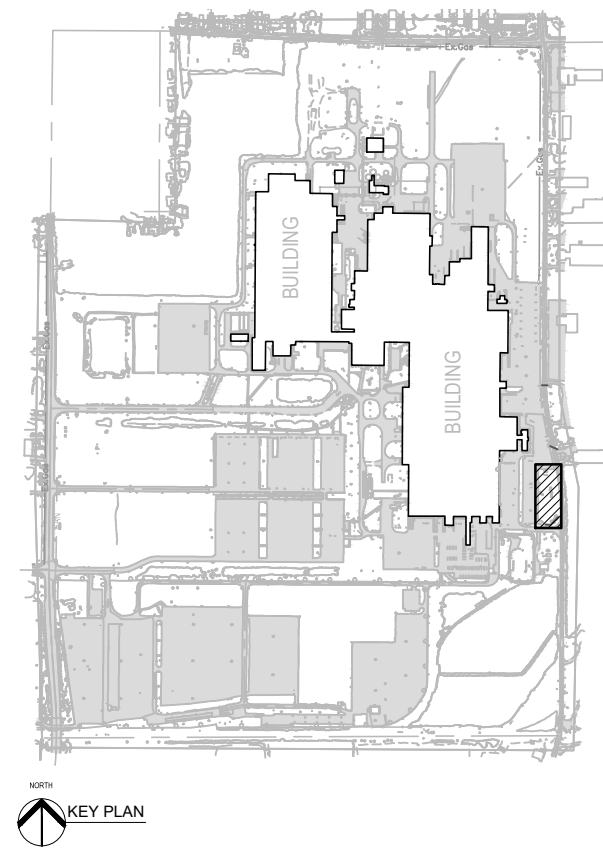
APPROVED BY: F. LAST M  
A/E PROJECT NUMBER: 30203632

SHEET TITLE  
STORMWATER DETENTION POND  
OUTLET PLAN AND PROFILE

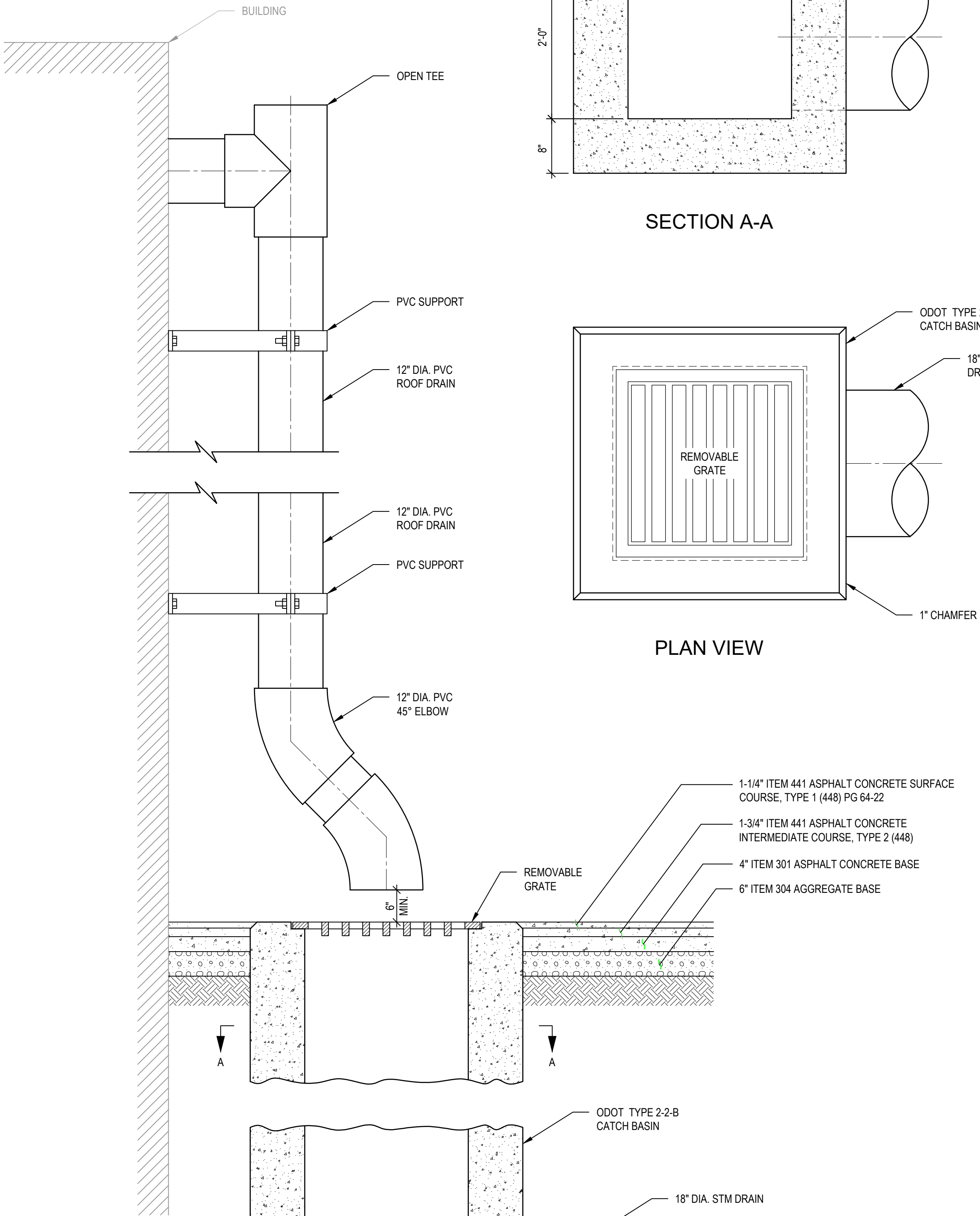
SHEET NUMBER

C-008





AVE TEAM INFORMATION	
IN CHARGE:	F. LAST NAME
DRAWN BY:	M. WELSHANS
DESIGNED BY:	P. OTIS
CHECKED BY:	P. OTIS
APPROVED BY:	F. LAST NAME
AVE PROJECT NUMBER:	<b>30203632</b>
SHEET TITLE	
<b>STORMWATER DETENTION POND SECTIONS</b>	
SHEET NUMBER	
<b>C-010</b>	

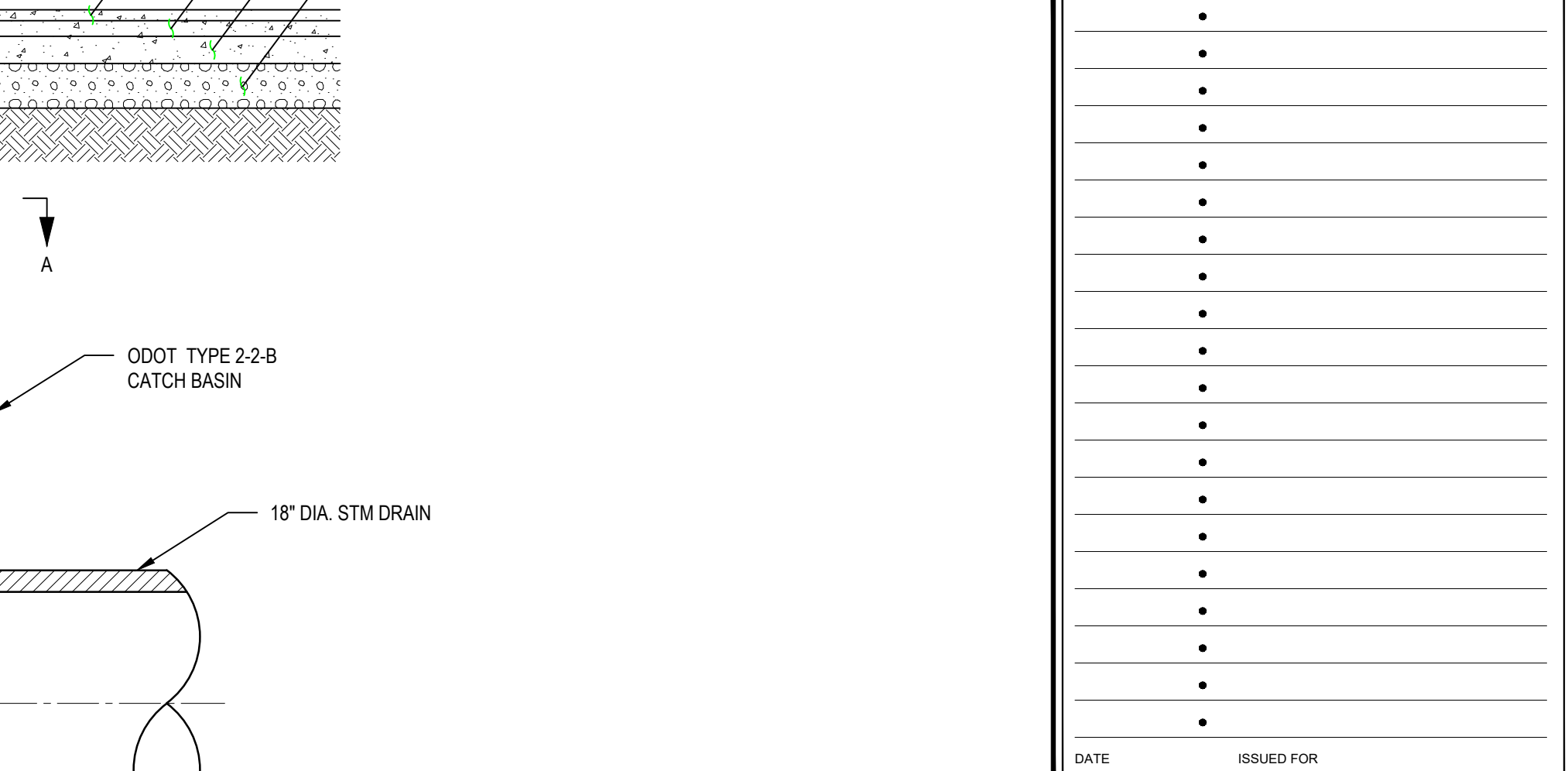


NOT TO SCALE

1. CONCRETE SHALL BE TAMPED AND RODDED IN PLACE.
2. FENCE FABRIC SHALL BE ZINC-COATED STEEL FABRIC, ASTM A392 HOT DIPPED GALVANIZED BEFORE OR AFTER WEAVING.
3. CONCRETE FOUNDATION ABOVE GRADE TO BE SLOPED OR DOMED AT THE TOP TO SHED WATER AWAY FROM THE POSTS.
4. SPECIFICATIONS PER FORD LAND SPECIFICATION SECTION 32 21 00-2



NOT TO SCALE



A/E TEAM INFORMATION	
IN CHARGE:	F. LAST NAME
DRAWN BY:	M. WELSHANS
DESIGNED BY:	P. OTIS
CHECKED BY:	P. OTIS
APPROVED BY:	F. LAST NAME
A/E PROJECT NUMBER:	<b>30203632</b>
SHEET TITLE <b>STORMWATER DETENTION POND          DETAILS - I</b>	
SHEET NUMBER	
C-010	

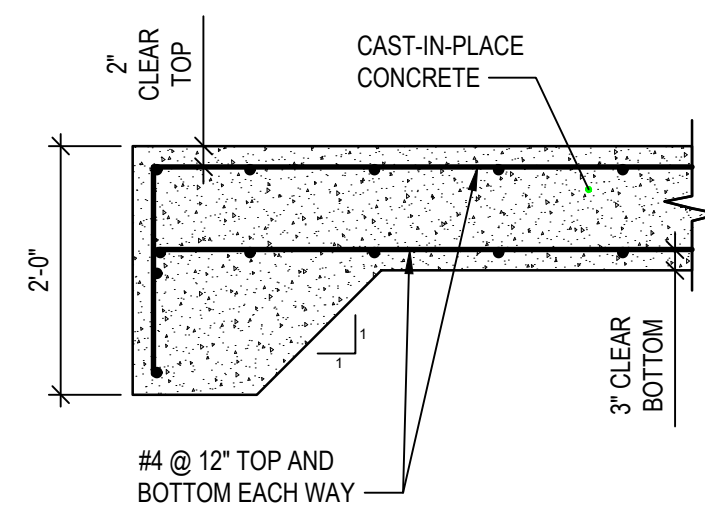


Diagram illustrating the cross-section of a retaining wall structure, showing various layers and dimensions:

- CAST-IN-PLACE CONCRETE** (Top and Bottom)
- #4 @ 12" OC EACH FACE (TYP.)** (Reinforcement bars)
- WALL HEIGHT (H)**
- 2' CLEAR TOP (TYP.)**
- 2'-6" (MAX)** (Clearance dimension)
- 2' CLEAR TYP. AT WALL**
- FINAL GRADE**
- 6" - THICK (APPR) TOPSOIL LAYER**
- 1'-0"** (Dimension)
- ODOT #57 STONE BACKFILL**
- GENERAL FILL OR SUBBASE**
- 1'-0"** (Dimension)
- 6"** (Dimension)
- 3' CLEAR BOTTOM**
- #4 @ 12" TOP & BOTTOM EACH WAY (TYP.)** (Reinforcement bars)

GENERAL FILL (AS REQUIRED)

FINAL GRADE

6"-THICK (MIN.) TOPSOIL LAYER

30" Ø RCP

6"-THICK SUBBASE TOP AND BOTTOM OF PIPE

6"-THICK SUBBASE TOP AND BOTTOM OF PIPE

12"-THICK SUBBASE

SLAB TURNDOWN (SEE THIS PAGE)

RIP RAP TO TIE IN AS NEEDED

FINAL GRADE

GENERAL FILL

8 OZ./SY NON-WOVEN GEOTEXTILE (TYP.)

REMOVABLE ORIFICE PLATE SEE DETAIL 6

GALVANIZED STEEL PIPE SADDLE (SEE NOTE 4)

PERMANENT POOL ELEV. 612.0

3" DIA. GALVANIZED STEEL POST WITH THREADED END

FINISHED GRADE OF POND SIDESLOPE

NOT LESS THAN 4 TIMES THE O.D. OF POST 1'-0" MIN. (TYP.)

36"x36" INLET FRAME AND GRATE (SEE NOTE 2)

EL. 616.8

MANHOLE STEPS (SEE NOTE 3)

60" DIA. PRECAST CONCRETE MANHOLE (SEE NOTE 1)

60"

EL. 613.6

3"

1"

2"

3.5' MIN.

3' MIN.

9.6' SUMP

NO. 4 STONE

CONCRETE FOOTING

8"

12"

24" RCP

36"x36" INLET FRAME AND GRATE (SEE NOTE 2)

EL. 617.0

FINISHED GRADE

60" DIA. PRECAST CONCRETE MANHOLE (SEE NOTE 1)

24" PIPE OUT

MANHOLE STEPS (SEE NOTE 3)

2.0' SUMP

NO. 4 STONE

8"

12"

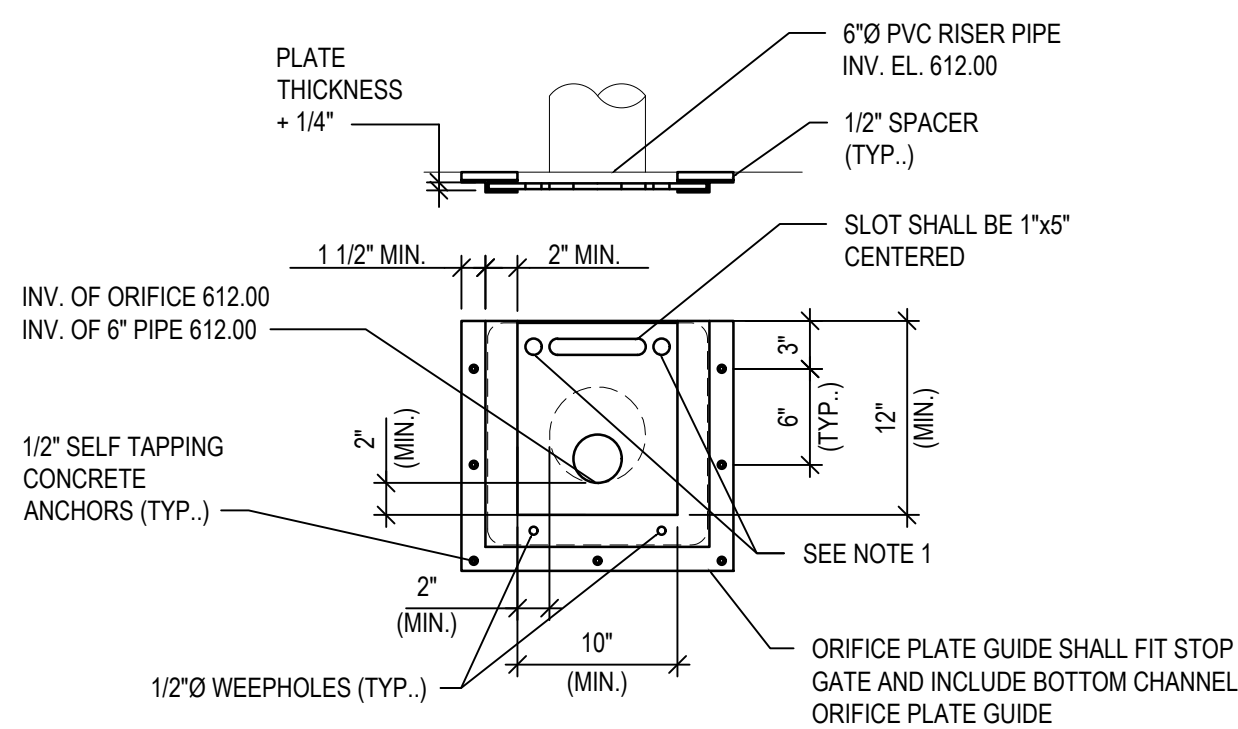
SECTION A-A'

Diagram illustrating the cross-section of a manhole structure, showing various components and elevations:

- 36"x36" INLET FRAME AND GRATE (SEE NOTE 2)**: Located at the top left of the structure.
- MANHOLE STEPS (SEE NOTE 3)**: Located at the top right of the structure.
- EL. 618.8**: Elevation of the top of the structure.
- 60"**: Internal width of the manhole structure.
- EL. 615.0**: Elevation of the top of the removable orifice plate.
- 0.7'W x 0.6'H ORIFICE**: Dimensions of the removable orifice plate.
- REMOVABLE ORIFICE PLATE SEE DETAIL X**: The plate itself, shown in a dashed circle.
- EL. 613.6**: Elevation of the top of the manhole structure.
- EL. 612.0**: Elevation of the top of the manhole structure.
- EL. 611.0**: Elevation of the top of the manhole structure.
- INV. OF 3" ORIFICE: 612.00**: Invert elevation of the 3-inch orifice.
- INV. OF 6" DIA. PVC PIPE: 612.00**: Invert elevation of the 6-inch diameter PVC pipe.
- 24" PIPE OUT**: Dimension of the pipe outlet.
- 2' SUMP**: Depth of the sump area.
- NO. 4 STONE**: Material used for the base of the structure.
- 6"**: Thickness of the stone base.
- 12"**: Total depth of the structure.

### OUTLET CONTROL STRUCTURE DETAIL 4

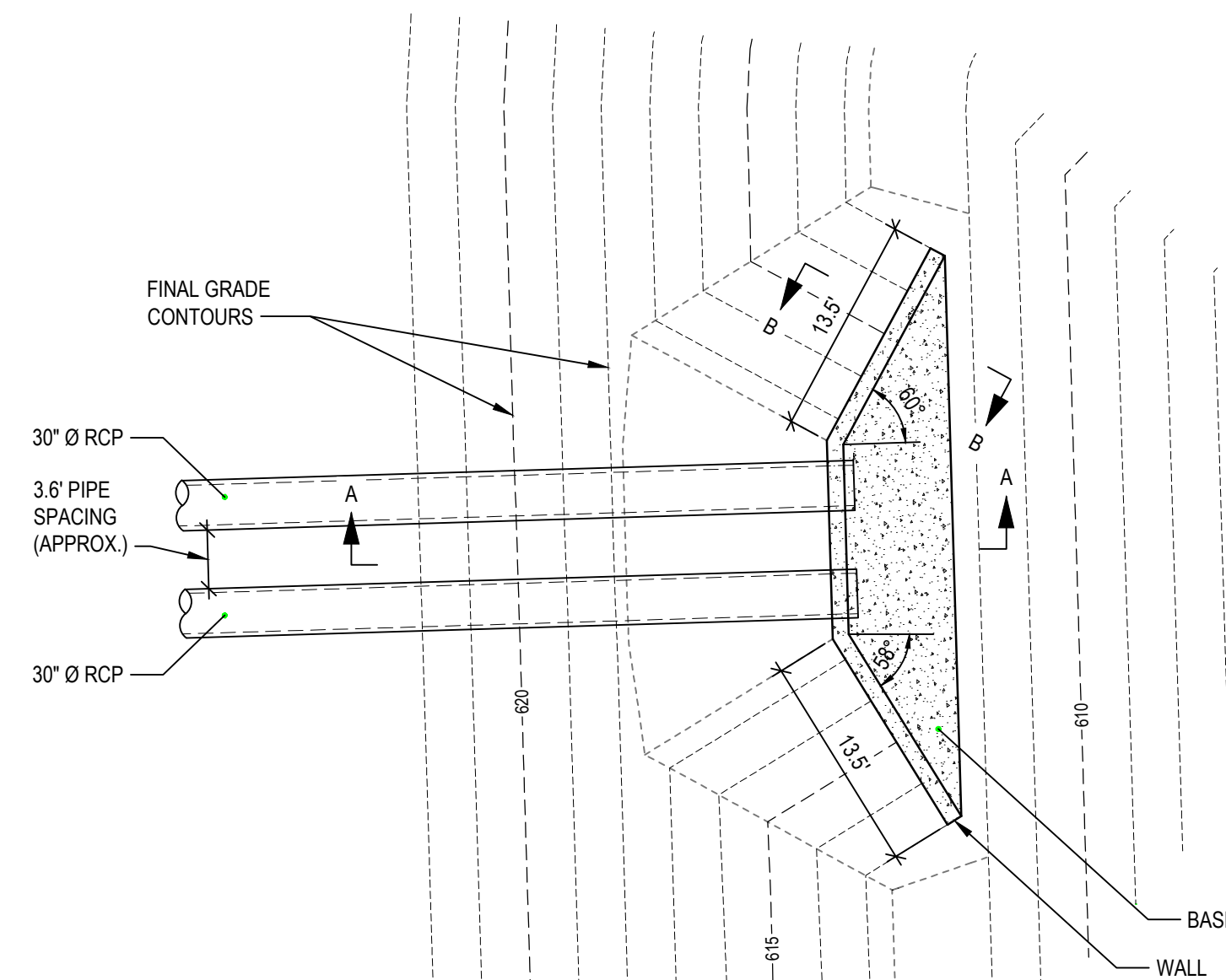
SCALE: 3/8" = 1'-0"



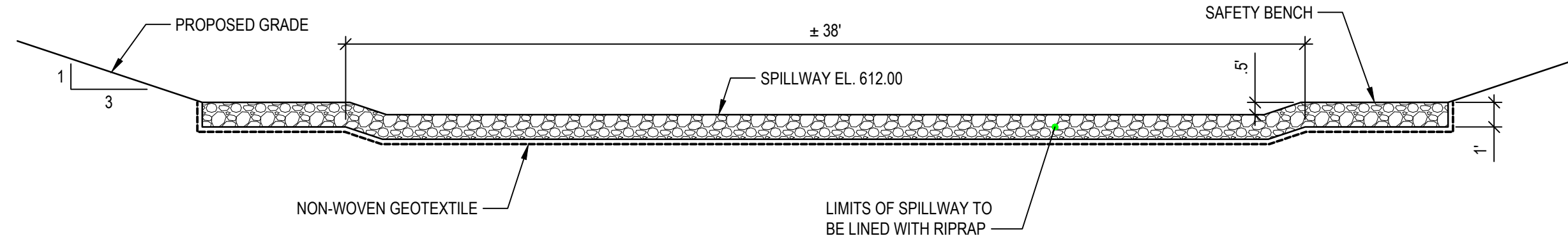
SCALE: 1" = 1'-0"

NOTE:

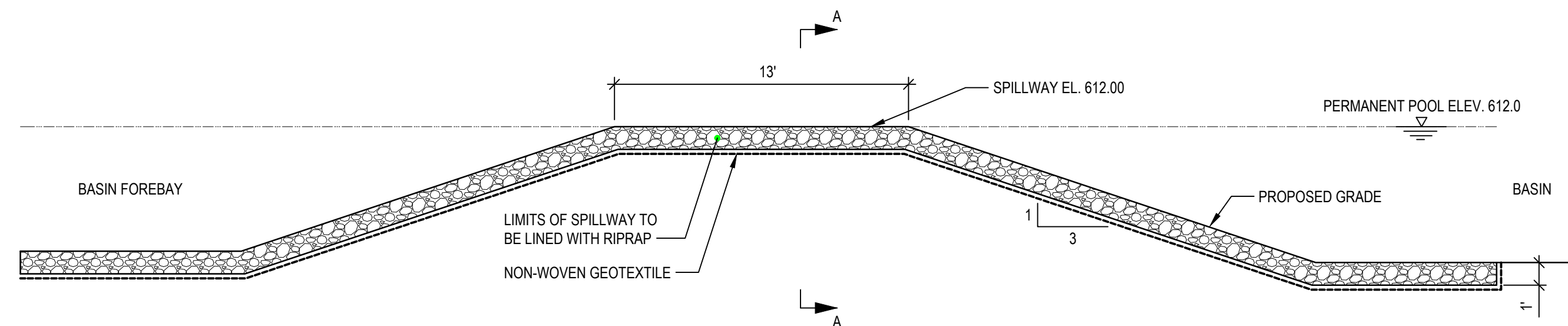
1. 3/16" STAINLESS STEEL CHAIN OR CABLE ATTACHED TO ORIFICE PLATE AND STRUCTURE AS APPROVED. CHAIN OR CABLE SHALL BE SMALL ENOUGH TO ALLOW ORIFICE PLATE TO BE REMOVED FROM GUIDE. ORIFICE PLATE AND GUIDE TO BE MANUFACTURED FROM 1/2" HDPE OR 1/4" STAINLESS STEEL.



NOT TO SCALE



SECTION A-A'



NOT TO SCALE



PRELIMINARY  
NOT FOR CONSTRUCTION

THIS DRAWING HAS BEEN PREPARED BY OR ON BEHALF OF FORD MOTOR COMPANY. FORD MOTOR COMPANY RETAINS ALL COMMON LAW, STATUTORY AND OTHER RESERVED RIGHTS INCLUDING COPYRIGHTS. THIS DRAWING SHALL NOT BE USED FOR ANY PURPOSE OTHER THAN PERFORMING SERVICES DIRECTLY OR INDIRECTLY TO FORD MOTOR COMPANY, WITHOUT THE EXPRESSED WRITTEN PERMISSION OF FORD MOTOR COMPANY. UNAUTHORIZED USE, COPYING OR MODIFICATION, INCLUDING THE REMOVAL OF THIS NOTICE, MAY CONSTITUTE A VIOLATION OF CIVIL OR CRIMINAL LAWS ENFORCEABLE BY FORD OR GOVERNMENTAL AGENCIES.

**COPYRIGHT © FORD MOTOR COMPANY (2005)**

DATE \_\_\_\_\_

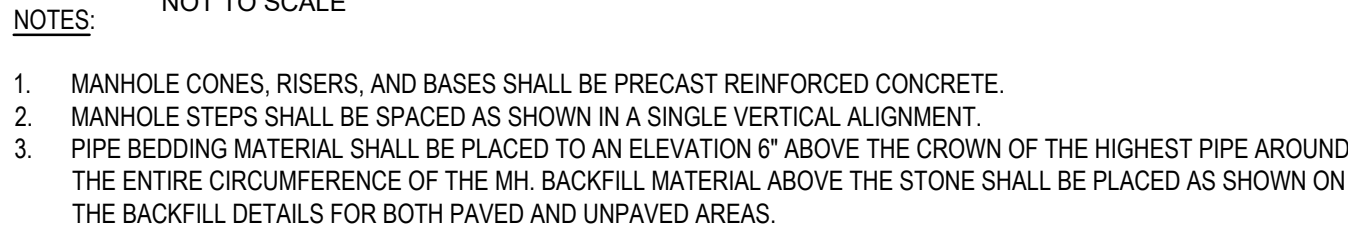
ISSUED FOR \_\_\_\_\_

A/E TEAM INFORMATION	
IN CHARGE:	F. LAST NAME
DRAWN BY:	M. WELSHANS
DESIGNED BY:	P. OTIS
CHECKED BY:	P. OTIS
APPROVED BY:	F. LAST NAME
A/E PROJECT NUMBER:	30203632
SHEET TITLE	
STORMWATER DETENTION POND DETAILS - II	
SHEET NUMBER	
C-011	



**NOTES:**

1. MATCH CONE, CHIMNEY, AND FRAME INSIDE DIAMETER.
2. BEVEL GRADE RINGS TO MATCH GRADE.
3. SELECT SIZE SO OUTSIDE DIAMETER EXTENDS TO OUTSIDE OF FRAME.



NOT TO SCALE



NOT TO SCALE



NOT TO SCALE



FORD LAND DOCUMENT PROJECT NUMBER: 3313-000



**PRELIMINARY**  
NOT FOR CONSTRUCTION

THIS DRAWING HAS BEEN PREPARED BY OR ON BEHALF OF FORD MOTOR COMPANY. FORD MOTOR COMPANY RETAINS ALL COMMON LAW, STATUTORY AND OTHER RESERVED RIGHTS, INCLUDING COPYRIGHTS. THIS DRAWING SHALL NOT BE USED FOR ANY PURPOSE OTHER THAN PERFORMING SERVICES DIRECTLY OR INDIRECTLY TO FORD MOTOR COMPANY, WITHOUT THE EXPRESSED WRITTEN PERMISSION OF FORD MOTOR COMPANY. UNAUTHORIZED USE, COPYING OR MODIFICATION, INCLUDING THE REMOVAL OF THIS NOTE, MAY CONSTITUTE A VIOLATION OF CIVIL OR CRIMINAL LAWS ENFORCEABLE BY FORD OR GOVERNMENTAL AGENCIES.

COPYRIGHT © FORD MOTOR COMPANY (2005)

A/E TEAM INFORMATION	
IN CHARGE:	F. LAST NAME
DRAWN BY:	M. WELSHANS
DESIGNED BY:	K. ASICHEMEIER
CHECKED BY:	K. ASICHEMEIER
APPROVED BY:	F. LAST NAME
A/E PROJECT NUMBER:	30203632
SHEET TITLE	
MISCELLANEOUS DETAILS - I	
SHEET NUMBER	
C-013	

